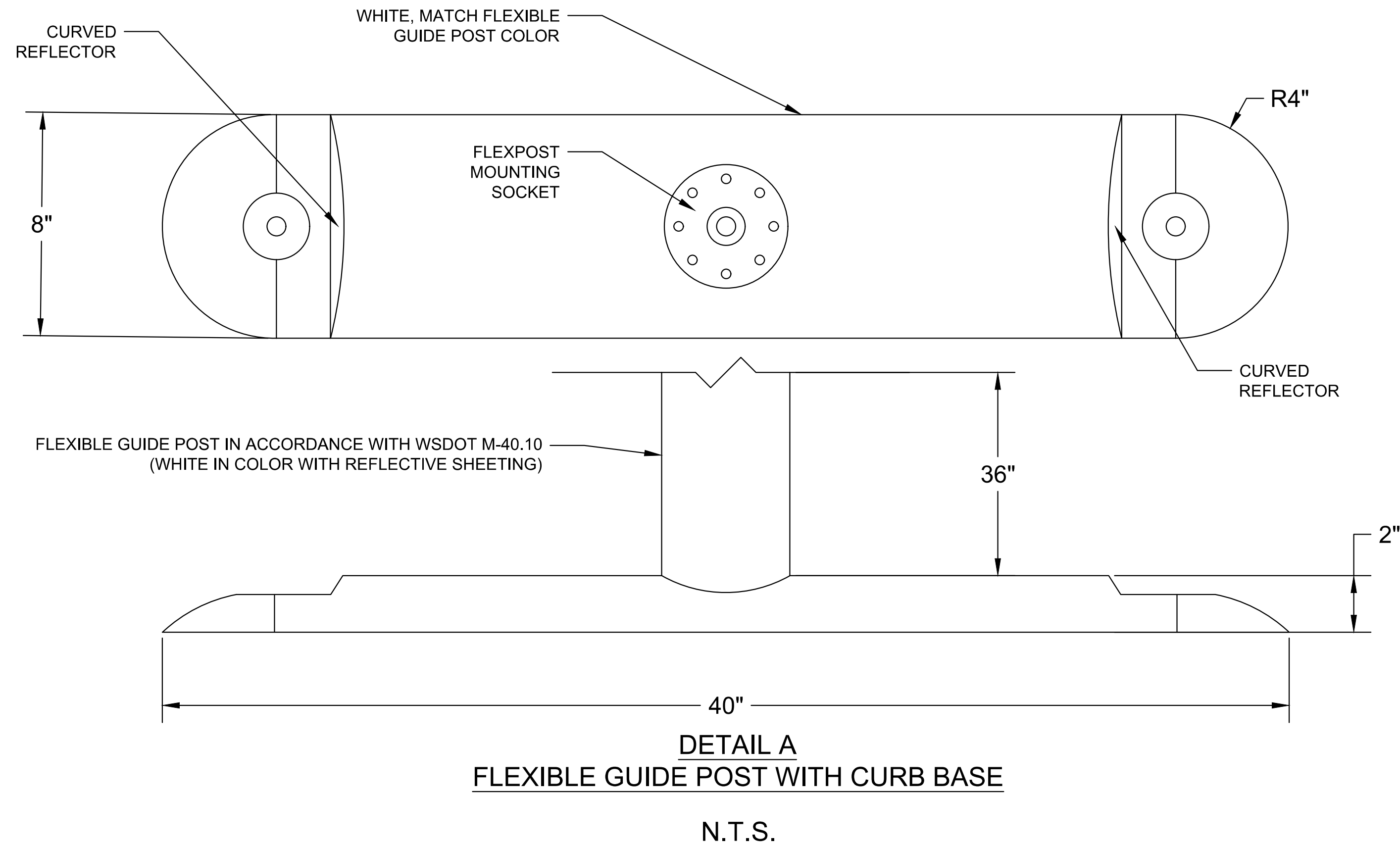


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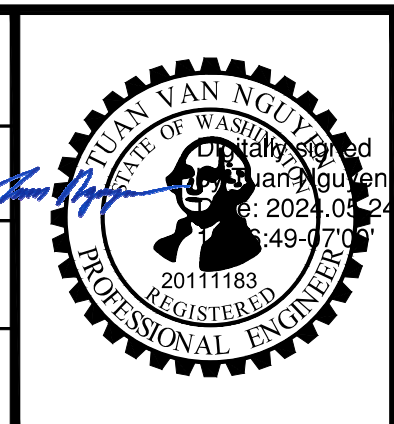
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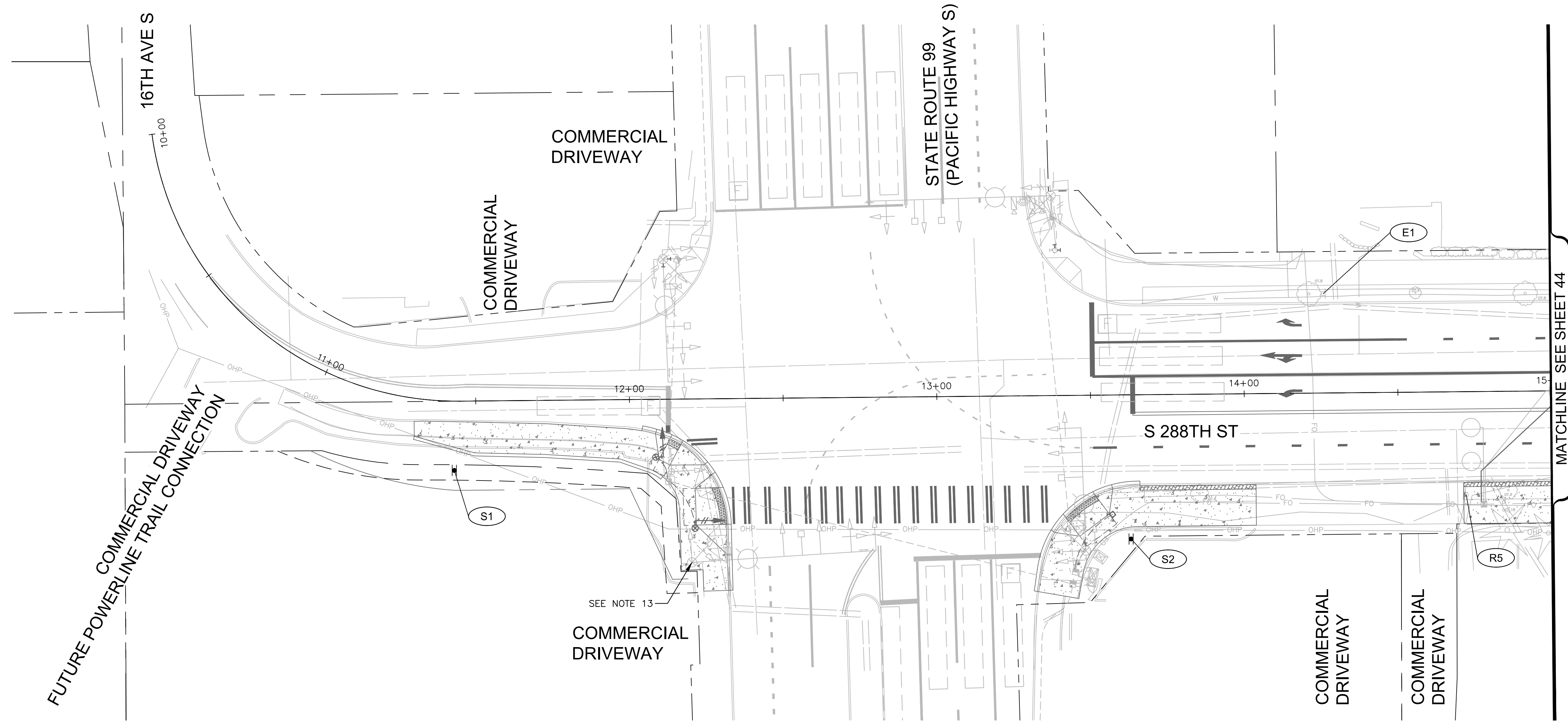
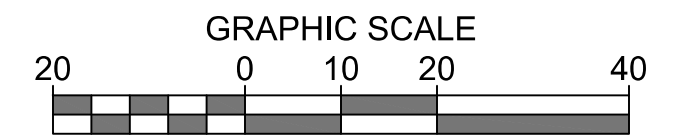
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S 288TH ST ROAD DIET - PHASE 2

PAVEMENT MARKINGS DETAILS

CITY PROJECT #: 36238
SHT. 42 OF 81

SEC.32/33 T.22N. R.4E. W.M.
SEC.4/5 T.21N. R.4E. W.M.



SIGNING GENERAL NOTES

- 1) ALL WORK SHALL BE ACCORDANCE WITH CITY OF FEDERAL WAY (COFW) STANDARD DRAWINGS AND SPECIFICATIONS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), AND THE CONTRACT UNLESS OTHERWISE NOTED.
- 2) EXISTING SIGNING THAT CONFLICTS WITH THE PROPOSED SIGNING, OR AS DIRECTED BY THE ENGINEER, SHALL BE REMOVED. EXISTING SIGNING TO REMAIN THAT IS DAMAGED OR OTHERWISE ALTERED BY CONSTRUCTION ACTIVITIES SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
- 3) THE CONTRACTOR SHALL SALVAGE AND DELIVER ALL EXISTING SIGNING EQUIPMENT SPECIFIED FOR REMOVAL BACK TO KING COUNTY, UNLESS OTHERWISE NOTED IN WRITING BY THE ENGINEER.
- 4) THE CONTRACTOR SHALL FIELD STAKE AND VERIFY FINAL LOCATIONS OF ALL PROPOSED SIGNS WITH THE ENGINEER PRIOR TO INSTALLATION.
- 5) RELOCATED SIGNS SHALL BE INSTALLED ON NEW POSTS IN ACCORDANCE WITH THE DETAIL IN THE PLANS (SEE SHEET 50).
- 6) OFFSET SIGN MINIMUM 2' FROM FACE OF CURB (WHEN PRESENT) UNLESS OTHERWISE NOTED.
- 7) ALL SIGN COLORS SHALL BE APPROVED BY THE CITY BEFORE INSTALLATION.
- 8) SIGNS INSTALLED AS PART OF TRAFFIC EQUIPMENT (I.E. ON SIGNAL POLES, RRFB, ELECTRONIC SPEED SIGN, FLASHING SCHOOL ZONE SIGN, ETC.) ARE INCLUDED AS A PART OF THOSE RESPECTIVE TRAFFIC BID ITEMS. THEY MAY BE SHOWN ON THE SIGN PLANS AND/OR IN THE SIGN TABLE FOR REFERENCE ONLY.
- 9) NEW AND RELOCATED SIGNS INSTALLED WITHIN SIDEWALK SECTIONS SHALL BE INSTALLED ON NEW POSTS PER WSDOT G-24.50-05, TYPE ST-4. NEW AND RELOCATED SIGNS INSTALLED OUTSIDE OF SIDEWALK SECTIONS SHALL BE INSTALLED WITH FOUNDATION IN ACCORDANCE WITH THE DETAIL IN THE PLANS (SEE SHEET 50).
- 10) ALL SIGNS ARE TO BE INSTALLED WITHIN PLANTER STRIPS OR BEHIND THE BACK OF SIDEWALK. IF NOT FEASIBLE, THEN SIGNS MAY BE INSTALLED WITHIN SIDEWALK SECTIONS. FOR SIGNS INSTALLED WITHIN SIDEWALK SECTIONS, BASE PLATE SHALL BE LOCATED IN PLANTER STRIP TO MAINTAIN MINIMUM 4FT CLEAR WIDTH FOR ADA.
- 11) THE LOCATION OF EXISTING FEATURES AND UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO COMMENCING SIGNING WORK. NOT ALL EXISTING FEATURES AND UTILITIES MAY BE SHOWN.
- 12) ALL WORK SHALL BE CONSISTENT WITH UTILITY AGENCY REQUIREMENTS. THE CONTRACTOR SHALL CONTACT UTILITY AGENCIES 48 HOURS PRIOR TO COMMENCING WORK AND SHALL COORDINATE WITH ALL AFFECTED UTILITY AGENCIES THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE TO EXISTING UTILITIES RESULTING FROM CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL NOTIFY THE AFFECTED UTILITY AGENCY IMMEDIATELY UPON DAMAGE AND SHALL BE RESPONSIBLE FOR REPLACING DAMAGED EQUIPMENT TO THE SATISFACTION OF THE AFFECTED UTILITY AGENCY.
- 13) SEE TRAFFIC SIGNAL PLANS FOR EXISTING AND PROPOSED SIGNS ON TRAFFIC SIGNAL EQUIPMENT.

LEGEND

- EXISTING SIGN
- NEW SINGLE SIDED SIGN
- NEW DOUBLE SIDED SIGN
- NEW SIGN NOTE
- EXISTING SIGN TO BE REMOVED NOTE
- EXISTING SIGN TO BE RELOCATED NOTE
- EXISTING SIGN TO REMAIN NOTE
- EXISTING RIGHT-OF-WAY
- PROPOSED TEMPORARY CONSTRUCTION EASEMENT (TCE)

NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S1	D11-1	BIKE ROUTE	11+43.0	22.5' RT	24" X 18"	FOR EASTBOUND TRAFFIC.
	M6-3	THROUGH ARROW			12" X 9"	PER MUTCD SECTION 9B. MOUNT BELOW D11-1 SIGN. FOR EASTBOUND TRAFFIC.
	D11-1	BIKE ROUTE			24" X 18"	FOR WESTBOUND TRAFFIC.
	M6-3	THROUGH ARROW			12" X 9"	PER MUTCD SECTION 9B. MOUNT BELOW D11-1 SIGN. FOR WESTBOUND TRAFFIC.
S2	D11-1	BIKE ROUTE	13+63.0	47.0' RT	24" X 18"	FOR EASTBOUND TRAFFIC.
	M6-3	THROUGH ARROW			12" X 9"	PER MUTCD SECTION 9B. MOUNT BELOW D11-1 SIGN. FOR EASTBOUND TRAFFIC.
	D11-1	BIKE ROUTE			24" X 18"	FOR WESTBOUND TRAFFIC.
	M6-3	THROUGH ARROW			12" X 9"	PER MUTCD SECTION 9B. MOUNT BELOW D11-1 SIGN. FOR WESTBOUND TRAFFIC.

EXISTING SIGNS TO REMAIN					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
E1	CUSTOM	RIGHT TURN ON RED YIELD TO U-TURN (ARROW SYMBOL)	14+26.0	32.5' LT	

EXISTING SIGNS TO BE REMOVED					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
R5	R3-5R	RIGHT TURN ONLY	14+72.0	33.5' RT	

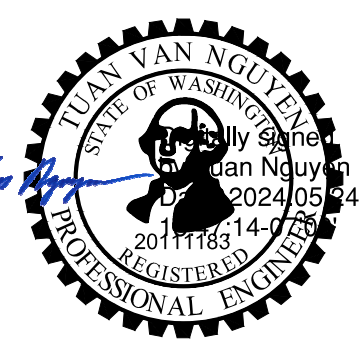


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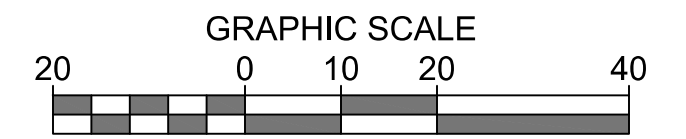
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CITY PROJECT #: 36238
SHT. 43
OF 81

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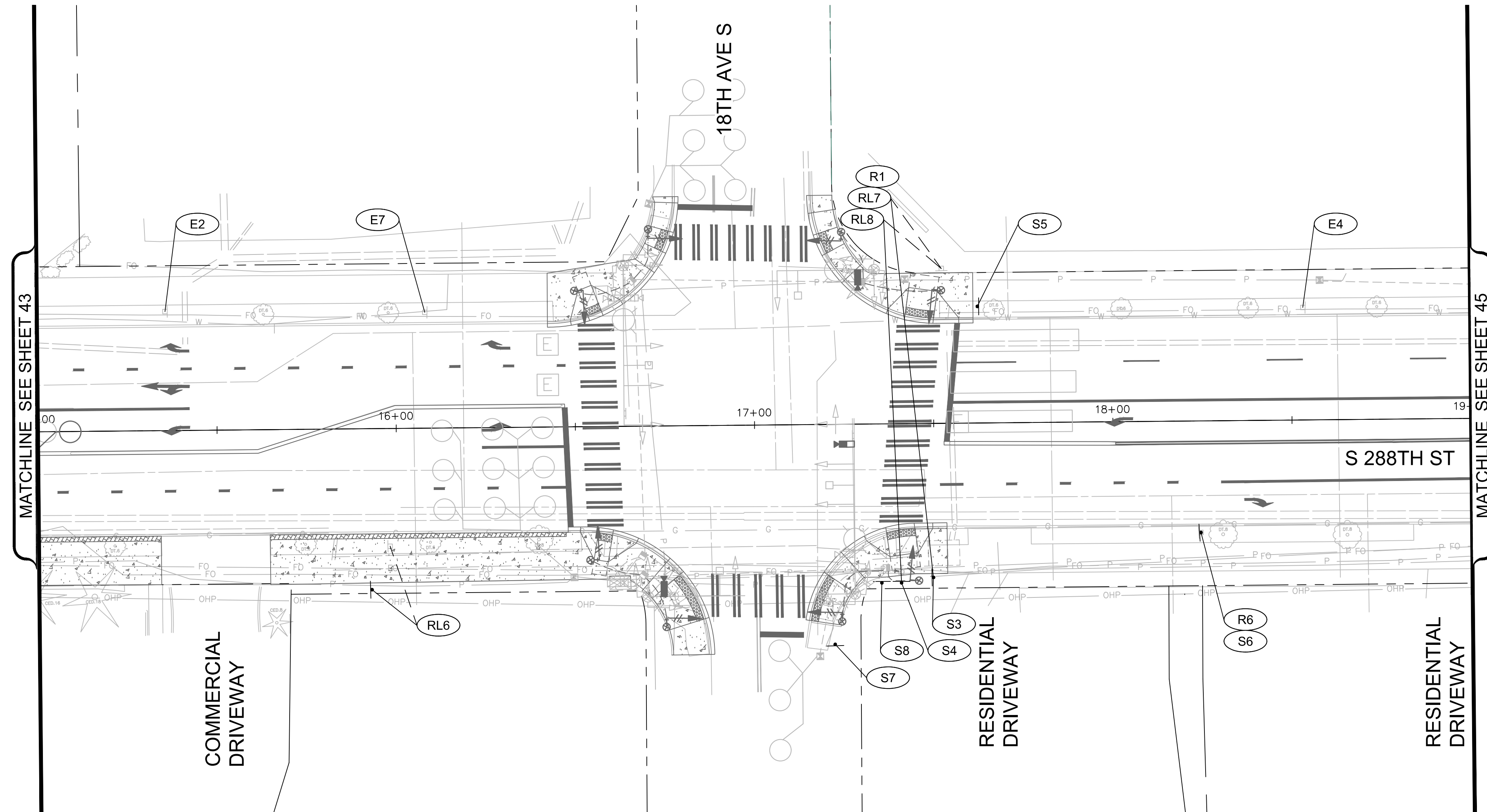
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SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.

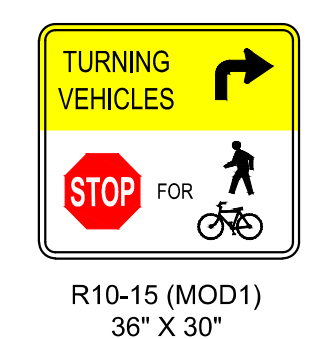


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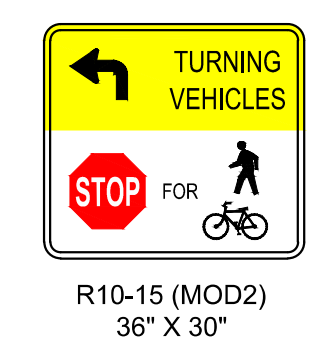
- 1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.



NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S3	M6-3	THROUGH ARROW	17+50.0	43.0' RT	12" X 9"	PER MUTCD SECTION 9B. MOUNT BELOW RELOCATED D11-1 SIGN.
S4	M6-4	BI-DIRECTIONAL ARROW	17+40.5	44.5' RT	12" X 9"	PER MUTCD SECTION 9B. MOUNT BELOW RELOCATED D11-1 SIGN.
S5	D11-1	BIKE ROUTE	17+62.5	32.5' LT	24" X 18"	PER MUTCD SECTION 9B. MOUNT BELOW D11-1 SIGN.
	M6-1L	DIRECTIONAL ARROW (LEFT)		12" X 9"		
S6	R4-4	BEGIN RIGHT TURN LANE YIELD TO BIKES	18+24.5	31.0' RT	36" X 30"	INSTALL ON STEEL POST.
S7	R10-15 (MOD1)	TURNING VEHICLES STOP FOR PEDESTRIAN BIKE RIGHT ARROW	17+22.0	61.5' RT	36" X 30"	SEE SIGN LEGEND BELOW.
S8	R10-15 (MOD2)	TURNING VEHICLES STOP FOR PEDESTRIAN BIKE LEFT ARROW	17+35.0	44.5' RT	36" X 30"	SEE SIGN LEGEND BELOW.



R10-15 (MOD1)
36" X 30"



R10-15 (MOD2)
36" X 30"

EXISTING SIGNS TO REMAIN					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
E2	R3-8A	LEFT, THRU/LEFT, RIGHT ARROW, AND ONLY	15+36.0	33.0' LT	
E4	R10-7	DO NOT BLOCK INTERSECTION	18+53.5	31.5' LT	
E7	R3-7R	RIGHT LANE MUST TURN RIGHT	16+08.5	32.5' LT	

EXISTING SIGNS TO BE REMOVED					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
R1	M6-1R	DIRECTIONAL ARROW (RIGHT)	17+52.5	43.0' LT	FOR NORTHBOUND TRAFFIC.
	M5-1R	RIGHT ARROW (90 DEGREES)			FOR WESTBOUND TRAFFIC.
R6	R2-1	SPEED LIMIT	18+24.5	31.0' RT	35MPH. REMOVE EXISTING WOOD POST.

EXISTING SIGNS TO RELOCATE							
SIGN NO.	SIGN CODE	DESCRIPTION	FROM		TO		NOTES
			STATION	OFFSET	STATION	OFFSET	
RL6	CUSTOM	LARGE TRUCKS PROHIBITED EXCEPT LOCAL DELIVERIES WEIGHT LIMIT 15 TONS	15+98.0	33.0' RT	15+93.0	45.0' RT	
RL7	D11-1	BIKE ROUTE	17+52.5	43.0' LT	17+50.0	43.0' RT	SEE SIGN S4.
RL8	D11-1	BIKE ROUTE	17+52.5	43.0' LT	17+40.5	43.5' RT	SEE SIGN S5.



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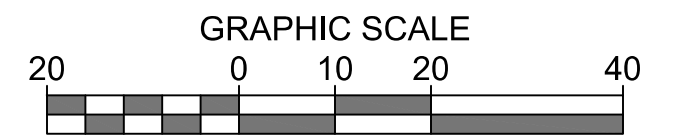
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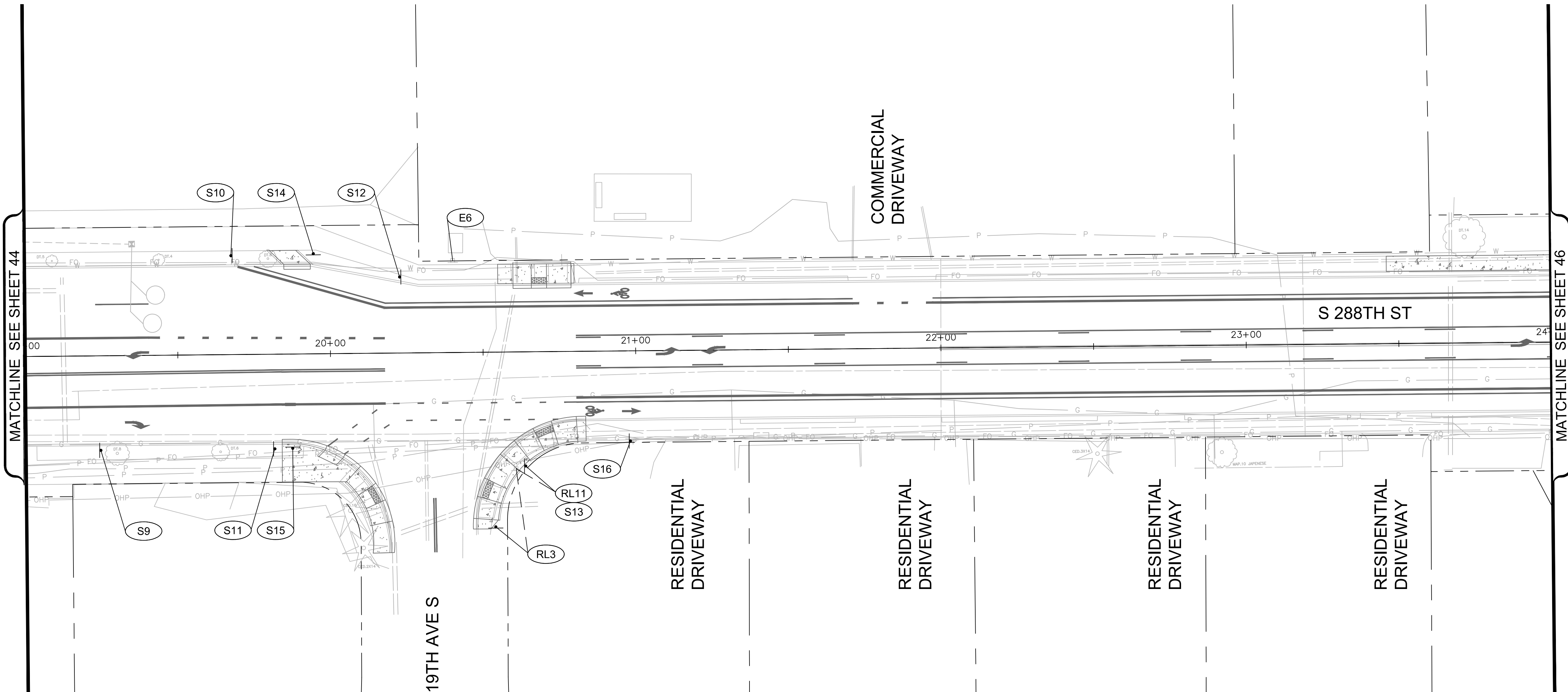
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SHT. **44**
OF **81**

SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.



NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S9	R3-7R	RIGHT LANE MUST TURN RIGHT	19+24.5	31.0' RT	30" X 30"	
S10	D11-1	BIKE ROUTE	19+67.5	32.5' LT	24" X 18"	MOUNT BELOW D11-1 SIGN.
	M6-2R	DIRECTIONAL ARROW (RIGHT)				
S11	D11-1	BIKE ROUTE	19+81.5	31.0' RT	24" X 18"	MOUNT BELOW D11-1 SIGN.
	M6-2L	DIRECTIONAL ARROW (LEFT)				
S12	R3-17	BIKE LANE	20+23.0	25.0' LT	24" X 18"	MOUNT BELOW R3-17 SIGN.
	R3-9BP	END				
S13	R3-17	BIKE LANE	20+63.5	37.5' RT	24" X 18"	MOUNT BELOW R3-17 SIGN.
	R3-9CP	BEGIN				

NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S14	R9-3A	NO PEDESTRIAN CROSSING	19+94.7	32.8' LT	12" X 18"	MOUNT BELOW R9-3A SIGN.
	R9-3BR	USE CROSSWALK (RIGHT)				
S15	R9-3A	NO PEDESTRIAN CROSSING	19+87.4	30.9' RT	12" X 18"	MOUNT BELOW R9-3A SIGN.
	R9-3BL	USE CROSSWALK (LEFT)				
S16	R2-1	SPEED LIMIT	20+98.0	29.5' RT	24" X 30"	30 MPH.

EXISTING SIGNS TO REMAIN						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES	
E6	D3-102	STREET NAME SIGN (S 288 ST)	20+40.5	30.0' LT		
	D3-102	STREET NAME SIGN (19 AVE S)				

EXISTING SIGNS TO RELOCATE							
SIGN NO.	SIGN CODE	DESCRIPTION	FROM		TO		NOTES
			STATION	OFFSET	STATION	OFFSET	
RL3	R1-1	STOP	20+60.5	38.0' RT	20+53.5	57.0' RT	
RL11	D3-102	STREET NAME SIGN (19 AVE S)	20+60.5	38.0' RT	20+63.5	37.5' RT	MOUNT ABOVE R3-17 SIGN.

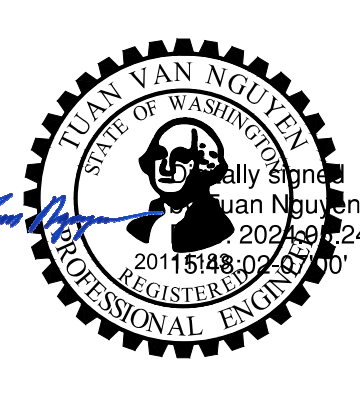


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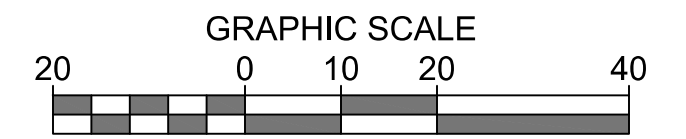
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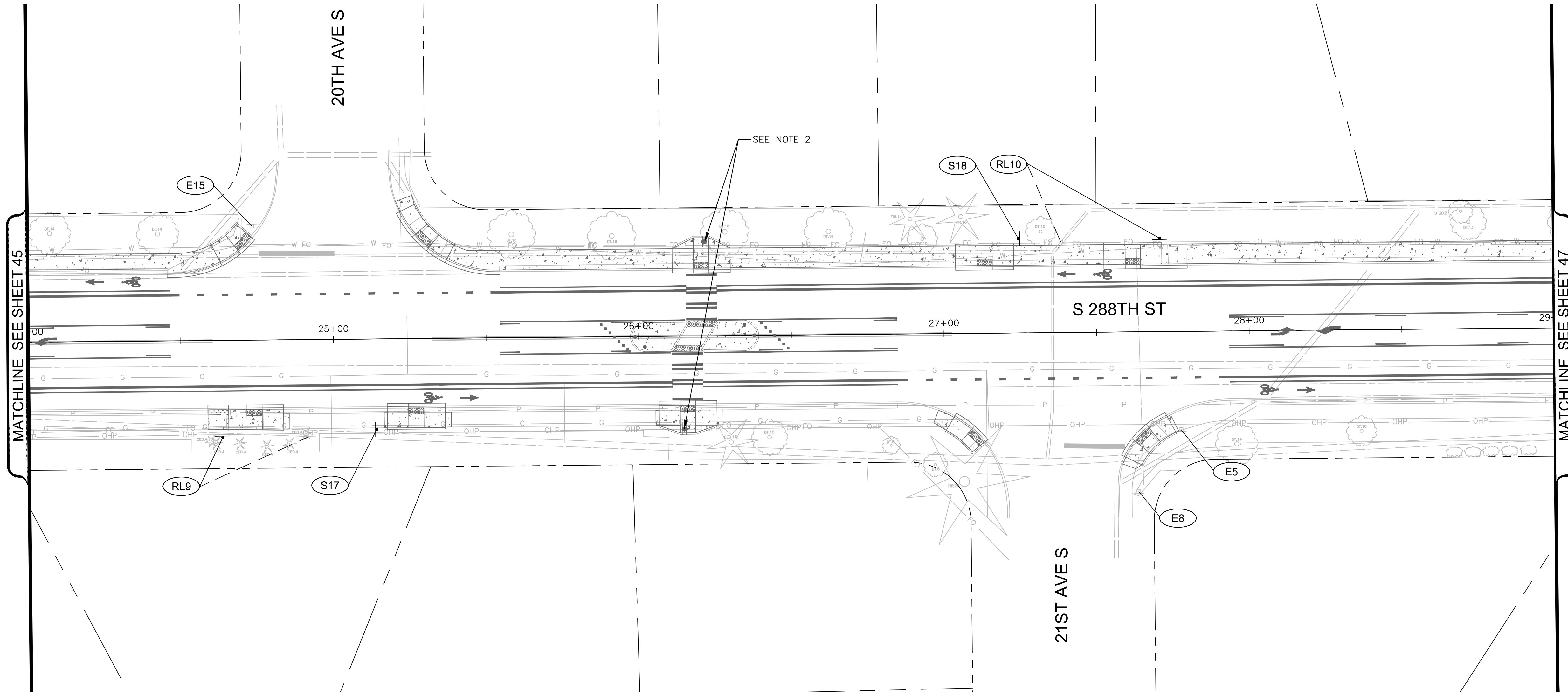
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SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



- 1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.
- 2) SEE SHEET 51 FOR RRFB PLAN AND DETAILS.



NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S17	W11-2	PEDESTRIAN	25+14.0	29.5' RT	30" X 30"	
S18	W11-2	PEDESTRIAN	27+24.5	31.0' LT	30" X 30"	

EXISTING SIGNS TO REMAIN					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
E5	D3-102	STREET NAME SIGN (21 AVE S)	27+77.5	32.0' RT	
E8	R1-1	STOP	27+63.5	52.5' RT	
	D11-3	BIKE ROUTE			
E15	R1-1	STOP	24+73.5	37.5' LT	FOR SOUTHBOUND.
	M6-1R	DIRECTIONAL ARROW (RIGHT)			FOR EASTBOUND.
	D11-3	BIKE ROUTE			
	M6-1L	DIRECTIONAL ARROW (LEFT)			

EXISTING SIGNS TO RELOCATE							
SIGN NO.	SIGN CODE	DESCRIPTION	FROM		TO		NOTES
			STATION	OFFSET	STATION	OFFSET	
RL9	D3-102	STREET NAME SIGN (S 288 ST)	24+91.5	32.0' RT	24+63.5	32.0' RT	
	D3-102	STREET NAME SIGN (20 AVE S)					
RL10	D3-102	STREET NAME SIGN (S 288 ST)	27+38.5	30.0' RT	27+71.0	30.5' RT	
	D3-102	STREET NAME SIGN (21 AVE S)					



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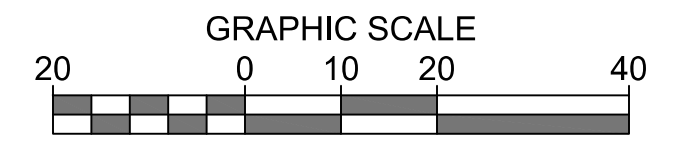
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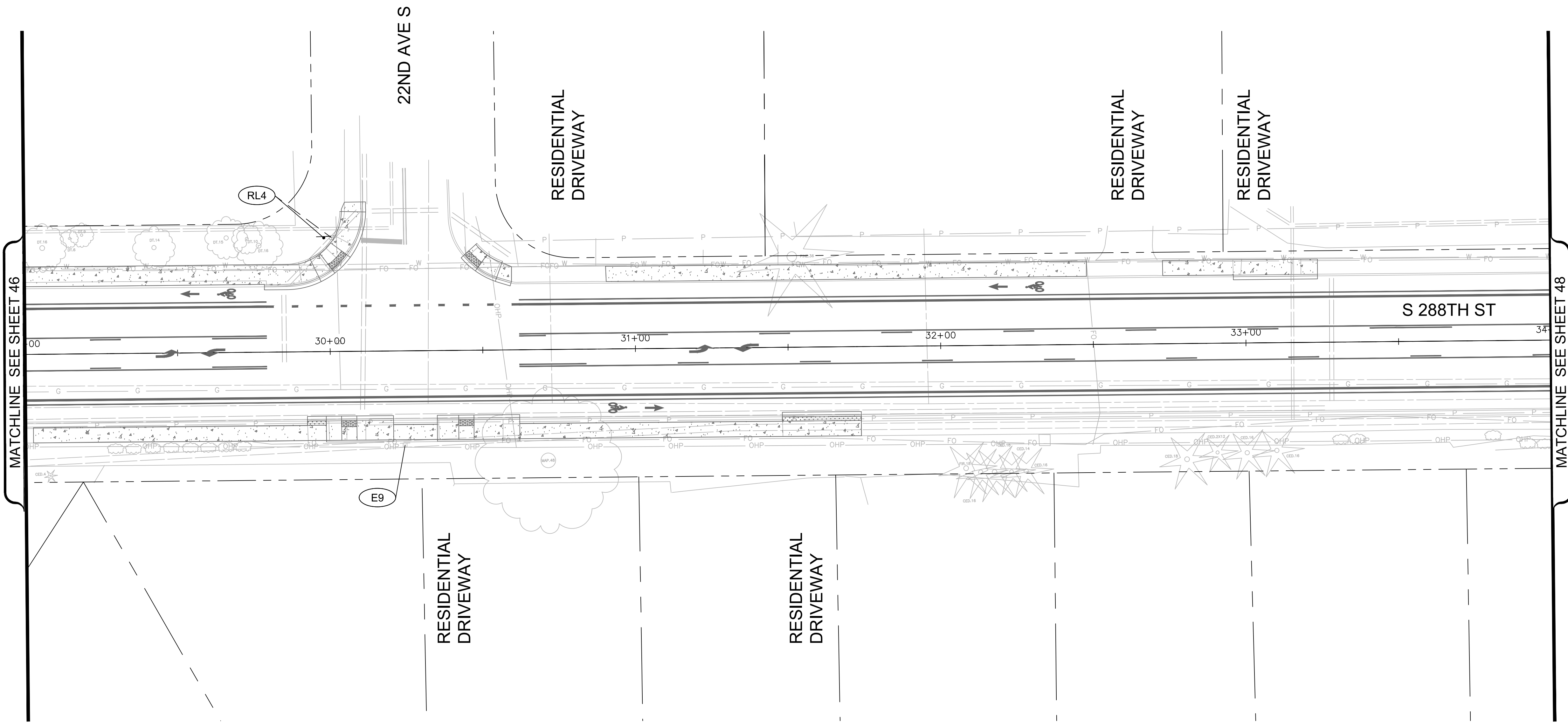
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OF **81**

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SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.



MATCHLINE SEE SHEET 46

MATCHLINE SEE SHEET 48

EXISTING SIGNS TO REMAIN							
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES		
E9	D3-102	STREET NAME SIGN (S 288 ST)	30+24.5	31.0' RT			
	D3-102	STREET NAME SIGN (22 AVE S)					
EXISTING SIGNS TO RELOCATE							
SIGN NO.	SIGN CODE	DESCRIPTION	FROM		TO		NOTES
			STATION	OFFSET	STATION	OFFSET	
RL4	D3-102	STREET NAME SIGN (22 AVE S)	30+00.5	37.5' LT	29+98.0	37.5' LT	
	D3-102	STREET NAME SIGN (S 288 ST)					
	R1-1	STOP					



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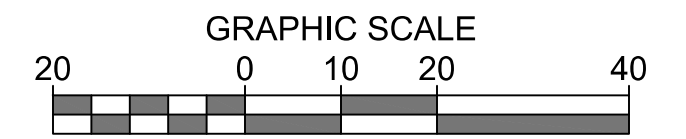
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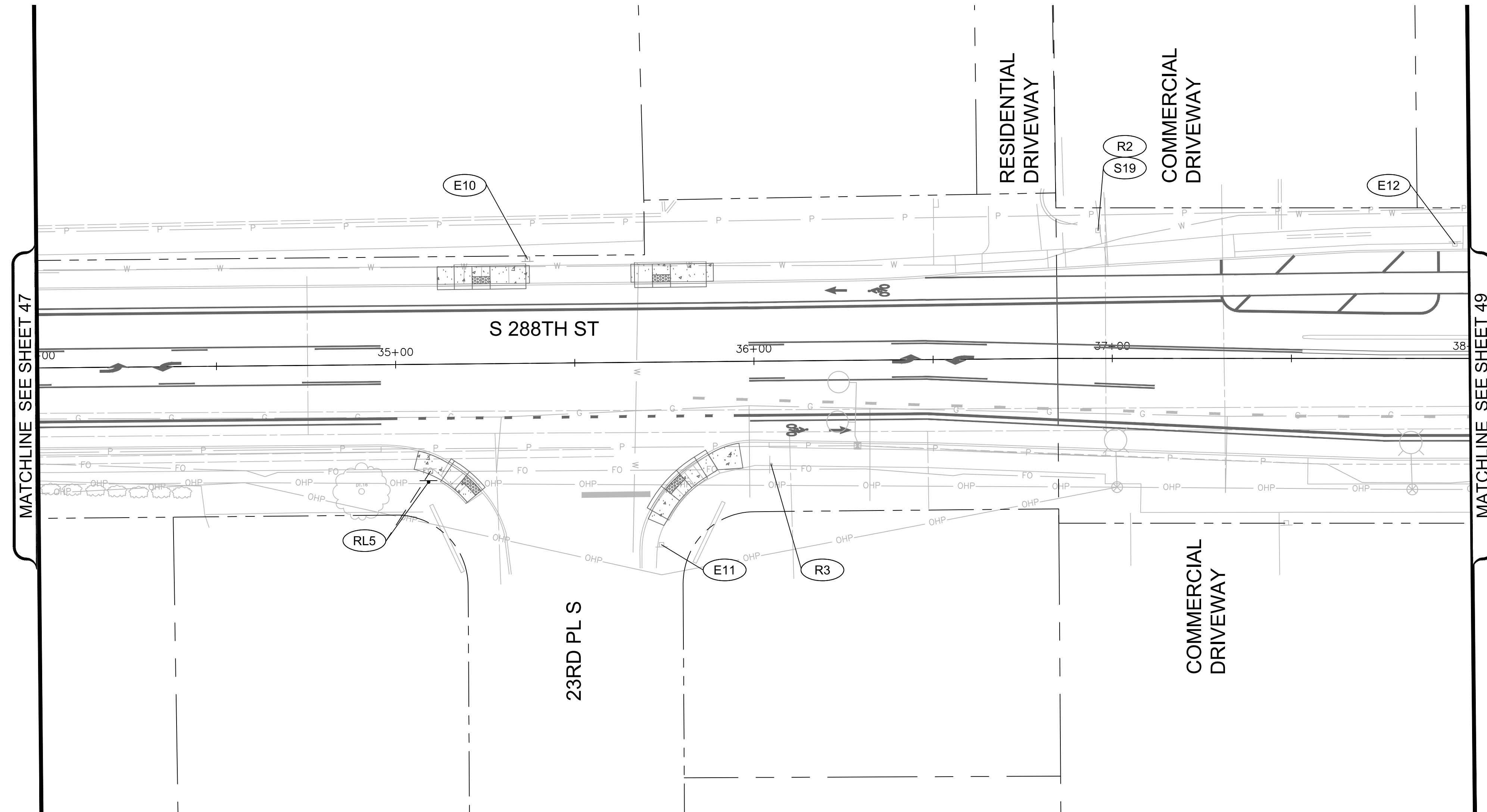
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SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.



NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S19	R2-1	SPEED LIMIT	36+96.0	35.5' LT	24" X 30"	30MPH. INSTALL ON STEEL POST.

EXISTING SIGNS TO REMAIN					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
E10	D3-102	STREET NAME SIGN (S 288 ST)	35+37.0	29.0' LT	
	D3-102	STREET NAME SIGN (23 PL S)			
E11	R1-1	STOP	35+73.5	51.0' RT	
E12	R3-5R	RIGHT TURN ONLY	37+95.5	32.0' LT	

EXISTING SIGNS TO BE REMOVED					
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES
R2	R2-1	SPEED LIMIT	36+96.0	35.5' LT	35 MPH.
R3	R3-7R	RIGHT LANE MUST TURN RIGHT	36+04.5	29.0' RT	

EXISTING SIGNS TO RELOCATE							
SIGN NO.	SIGN CODE	DESCRIPTION	FROM		TO		NOTES
			STATION	OFFSET	STATION	OFFSET	
RL5	D3-102	STREET NAME SIGN (S 288 ST)	35+09.5	30.0' RT	35+09.0	33.0' RT	
	D3-102	STREET NAME SIGN (23 PL S)					

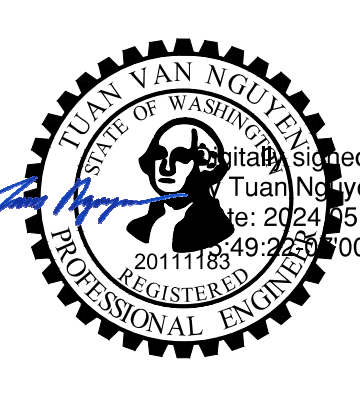


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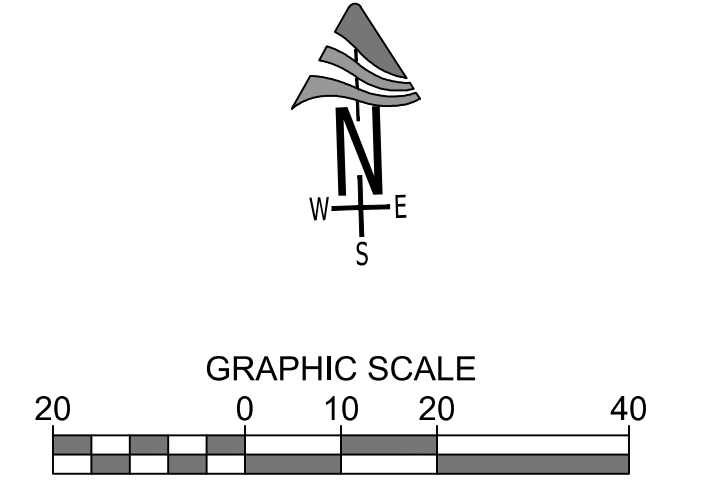
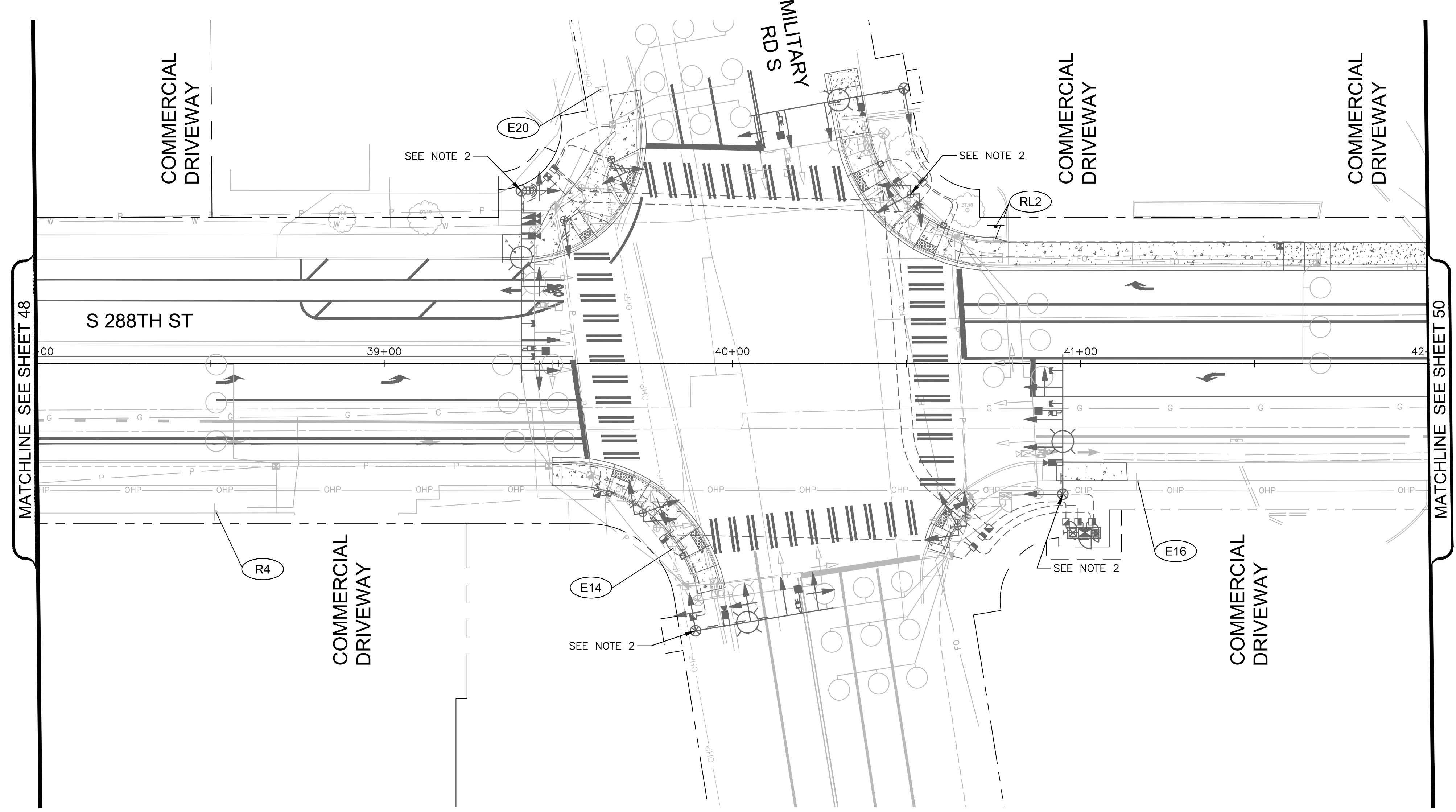
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SIGNING PLAN

CITY PROJECT #:
36238

SHT. **48**
OF **81**

SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



- 1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.
- 2) SEE TRAFFIC SIGNAL PLANS FOR EXISTING AND PROPOSED SIGNS ON TRAFFIC SIGNAL EQUIPMENT.

EXISTING SIGNS TO REMAIN							
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES		
E14	SPECIAL	CAMELOT ELEM. SCHOOL	39+83.0	53.0' RT			
	SPECIAL	THOMAS JEFFERSON HIGH SCHOOL					
E16	R2-1	SPEED LIMIT	41+16.5	34.0' RT	30 MPH.		
E20	R10-11	NO TURN ON RED	39+62.0	79.0' LT			
EXISTING SIGNS TO BE REMOVED							
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES		
R4	R3-7R	RIGHT LANE MUST TURN RIGHT	38+51.5	42.5' RT			
EXISTING SIGNS TO RELOCATE							
SIGN NO.	SIGN CODE	DESCRIPTION	FROM		TO		NOTES
			STATION	OFFSET	STATION	OFFSET	
RL2	R3-5R	RIGHT TURN ONLY	40+75.5	35.5' LT	40+75.5	39.5' LT	



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S 288TH ST ROAD DIET - PHASE 2

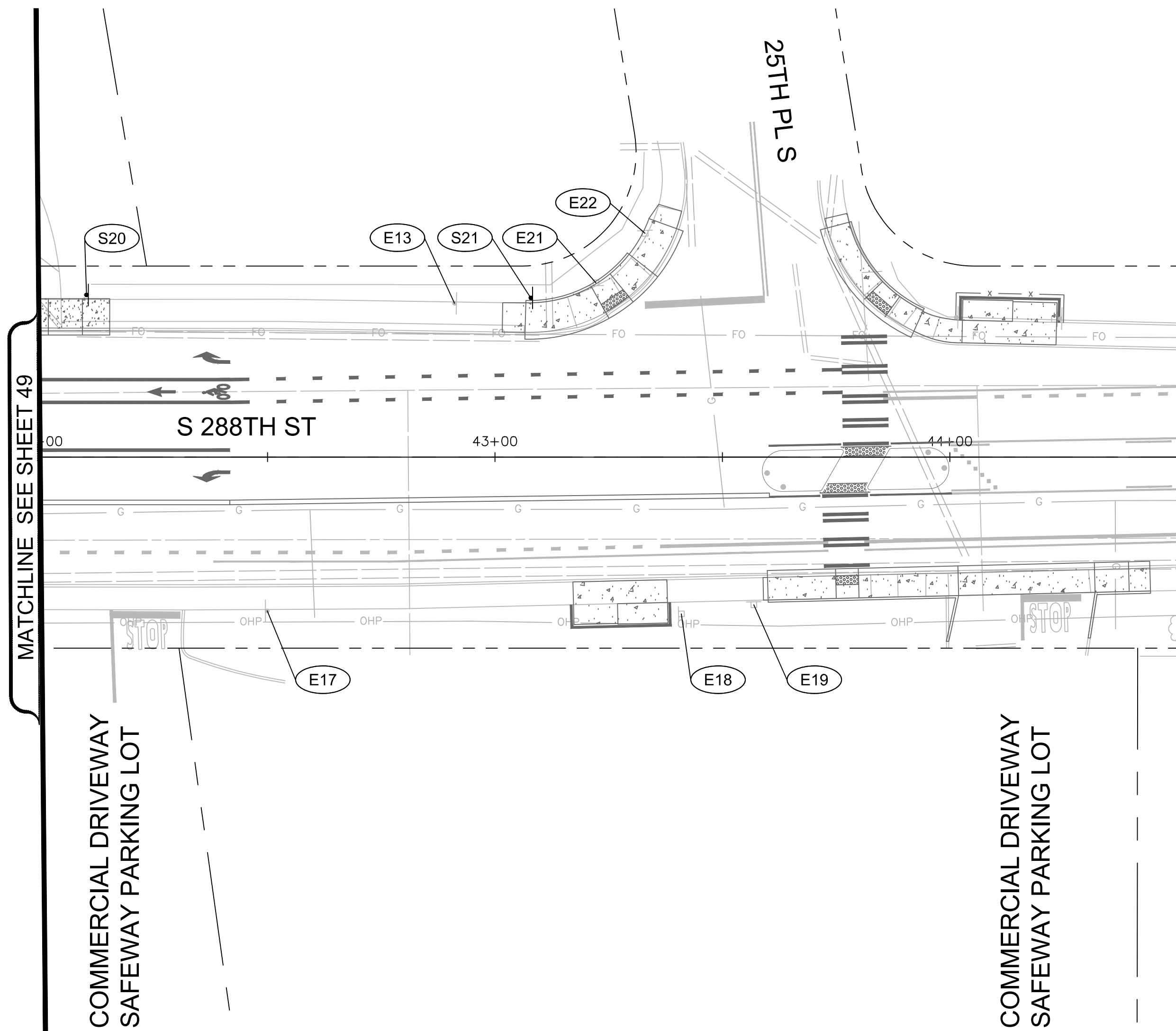
SIGNING PLAN

CITY PROJECT #:
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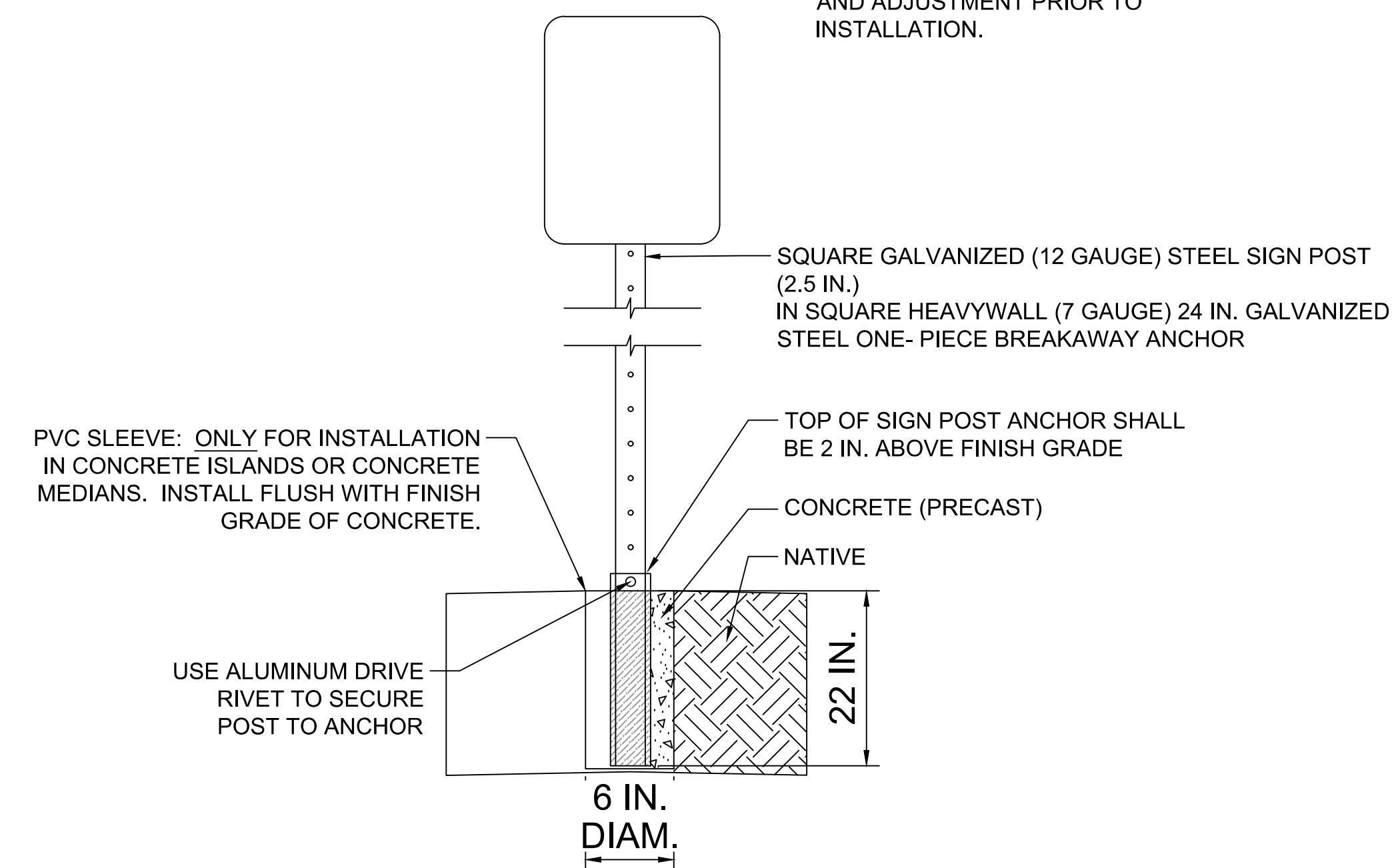
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NOTES

- 1) MOUNTING HEIGHT SHALL BE IN ACCORDANCE WITH THE CURRENT ADOPTED EDITION OF THE MUTCD.
- 2) FOR STEEL POST INSTALLATIONS, USE A NYLON WASHER AND AN ALUMINUM DRIVE RIVET TO INSTALL THE SIGN TO THE POST.
- 3) VERIFY AND STAKE LOCATIONS OF TRAFFIC SIGNS FOR ENGINEER APPROVAL AND ADJUSTMENT PRIOR TO INSTALLATION.



GALVANIZED SIGN POST DETAIL

1) SEE SHEET 43 FOR SIGNING GENERAL NOTES AND LEGEND.

NEW SIGNS						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	SIZE	NOTES
S20	R3-7R	RIGHT LANE MUST TURN RIGHT	42+10.0	35.5' LT	30" X 30"	
S21	R4-4	BEGIN RIGHT TURN LANE YIELD TO BIKES	43+08.0	35.0' LT	36" X 30"	

EXISTING SIGNS TO REMAIN						
SIGN NO.	SIGN CODE	DESCRIPTION	STATION	OFFSET	NOTES	
E13	W3-3	TRAFFIC SIGNAL	42+91.0	34.0' LT		
	CUSTOM	MILITARY RD S				
E17	W11-2	PEDESTRIAN	42+50.0	34.0' RT		
E18	CUSTOM	KING COUNTY METRO STOP	43+41.0	34.5' RT		
E19	D3-102	STREET NAME SIGN (S 288 ST)	43+57.0	32.5' RT		
	D3-102	STREET NAME SIGN (25 PL S)				
E21	D3-102	STREET NAME SIGN (25 PL S)	43+22.0	38.5' LT		
E22	R1-1	STOP	43+33.0	49.0' LT		



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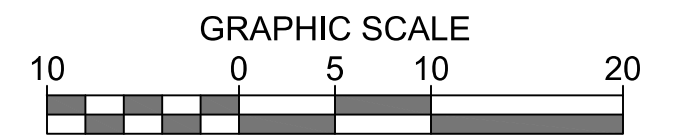
S 288TH ST ROAD DIET - PHASE 2

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OF **81**

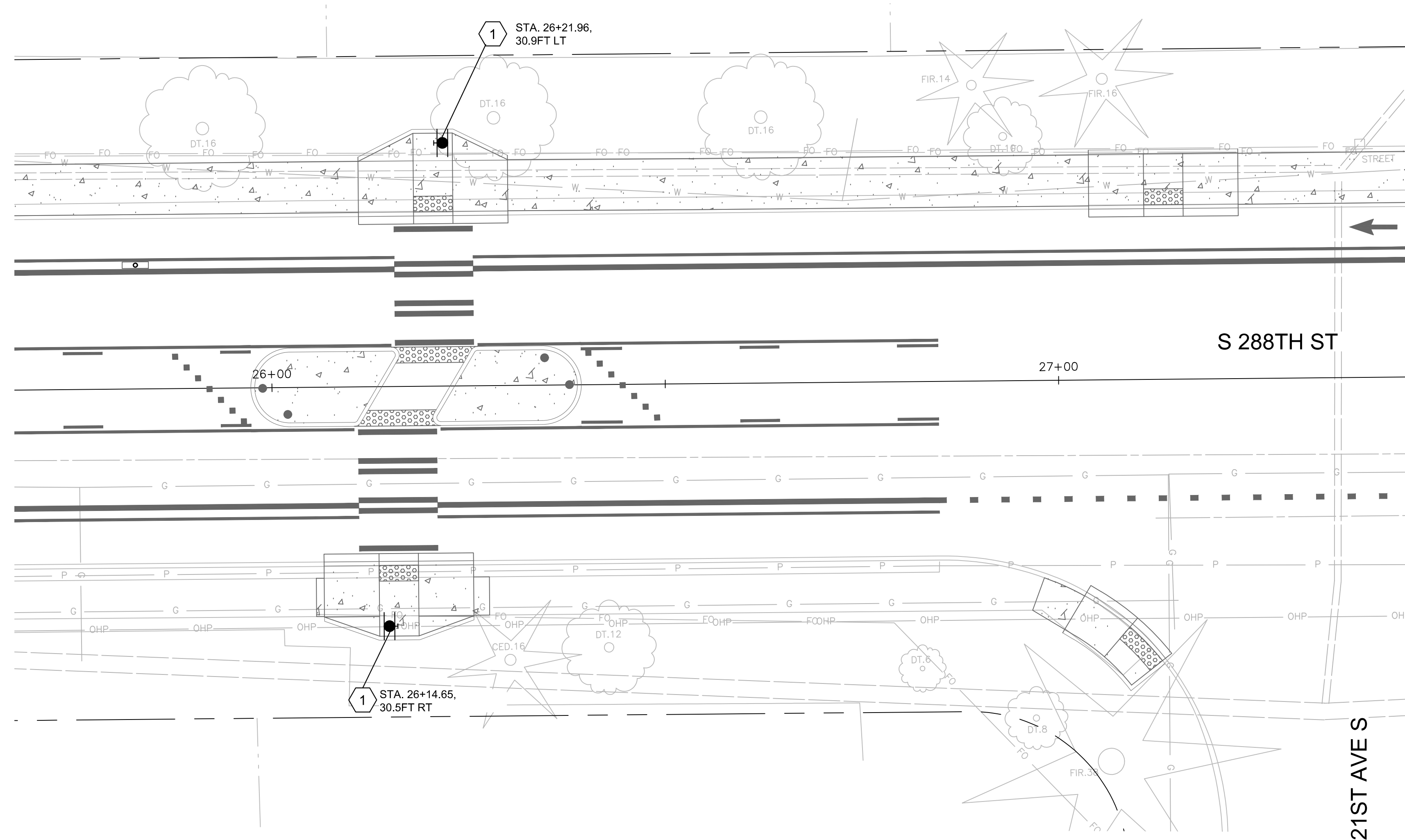
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SEC.33 T.22N. R.4E. W.M.
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RRFB GENERAL NOTES

1. THE CONTRACTOR SHALL FIELD VERIFY AND STAKE FINAL LOCATIONS OF ALL PROPOSED RRFB SIGNS AND FOUNDATIONS WITH THE ENGINEER PRIOR TO INSTALLATION. ALL PROPOSED SIGNS AND FOUNDATIONS SHALL BE LOCATED WITHIN RIGHT-OF-WAY AND PANELS SHALL NOT OVERHANG OUTSIDE OF RIGHT-OF-WAY. SIGN PANELS THAT OVERHANG SIDEWALKS/PATHWAYS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 7 FT FROM THE BOTTOM OF THE LOWEST PANEL TO THE ADJACENT SIDEWALK/PATHWAY SURFACE.
2. SIGN CODES REFER TO THE MUTCD.
3. ROADSIDE W16-7P SIGN SHALL HAVE ARROW POINT DOWNWARD TOWARDS THE DIRECTION OF THE CROSSWALK EITHER TO RIGHT (W16-7PR) OR TO THE LEFT (W16-7PL).
4. SEE PAVEMENT MARKING, SIGNING, PAVING, AND CURB RAMP PLANS FOR FURTHER DETAILS.
5. THE LOCATION OF EXISTING FEATURES AND UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO COMMENCING ELECTRICAL WORK. NOT ALL EXISTING FEATURES AND UTILITIES MAY BE SHOWN.
6. ALL WORK SHALL BE CONSISTENT WITH UTILITY AGENCY REQUIREMENTS. THE CONTRACTOR SHALL CONTACT UTILITY AGENCIES 48 HOURS PRIOR TO COMMENCING WORK AND SHALL COORDINATE WITH ALL AFFECTED UTILITY AGENCIES THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE TO EXISTING UTILITIES RESULTING FROM CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL NOTIFY THE AFFECTED UTILITY AGENCY IMMEDIATELY UPON DAMAGE AND SHALL BE RESPONSIBLE FOR REPLACING DAMAGED EQUIPMENT TO THE SATISFACTION OF THE AFFECTED UTILITY AGENCY.
7. POLE FOUNDATION LOCATIONS SHALL BE STAKED IN THE FIELD, POTHOLED TO VERIFY NO POTENTIAL UTILITY CONFLICTS EXIST, AND APPROVED BY THE ENGINEER PRIOR TO EXCAVATION.
8. THE CONTRACTOR SHALL CHECK FOR MINIMUM OVERHEAD CLEARANCE BETWEEN ANY OVERHEAD POWER AND COMMUNICATION LINES PRIOR TO PLACING FOUNDATIONS.
9. PEDESTRIAN PUSHBUTTON SHALL BE LOCATED WITHIN A 9-INCH REACH FROM CURB RAMP LANDING AREAS PER ADA ACCESSIBILITY GUIDELINES.
10. CONTRACTOR TO TRIM ANY TREE BRANCHES AND/OR VEGETATION TO PROVIDE VISIBILITY OF FLASHING RRFB UNIT(S) FROM THE TRAVELED WAY AND SKY VISIBILITY FOR SOLAR PANEL PER COFW DIRECTION.
11. SEE SHEET 52 FOR RECTANGULAR RAPID FLASHING BEACON POLE, AND FOUNDATION DETAILS.



LEGEND

- RRFB DOUBLE SIDED
- PEDESTRIAN PUSHBUTTON

CONSTRUCTION NOTES

- 1. INSTALL DOUBLE SIDED RECTANGULAR RAPID FLASHING BEACON (RRFB) POLE, PUSHBUTTON, SIGNS, BASE, FLASHERS, BASE COLLAR, SOLAR PANEL, AND FOUNDATION.



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S 288TH ST ROAD DIET - PHASE 2
RECTANGULAR RAPID FLASHING BEACON (RRFB) PLAN

CITY PROJECT #:	36238
SHT.	51
OF	81

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GENERAL NOTES

- ALL WORK SHALL BE IN ACCORDANCE WITH WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) STANDARD SPECIFICATIONS, CITY OF FEDERAL WAY STANDARDS, AND PROJECT SPECIFICATIONS.
- AS A PART OF THE DESIGN PROCESS, THE ENGINEER HAS ATTEMPTED TO CONFIRM THAT THERE ARE NO CONFLICTS WITH OVERHEAD POWER/COMMUNICATIONS LINES OR SUBSURFACE UTILITIES. HOWEVER, IT IS POSSIBLE FOR THE EXISTING OR PROPOSED CONDITIONS TO CHANGE PRIOR TO IMPLEMENTATION OF THE SCOPE OF WORK IDENTIFIED ON THESE DOCUMENTS. AS THE FIRST ORDER OF BUSINESS, THE CONTRACTOR SHALL POTHOLE EACH POLE LOCATION TO CONFIRM THAT THERE ARE NO CONFLICTS WITH SUBSURFACE UTILITIES. ADDITIONALLY, THE CONTRACTOR SHALL CONFIRM THAT THERE IS A MINIMUM 10-FOOT CLEARANCE PROVIDED BETWEEN OVERHEAD POWER LINES AND THE PROPOSED TRAFFIC SIGNAL EQUIPMENT, AND THAT NO OVERHEAD COMMUNICATIONS LINES WILL OBSTRUCT THE VISIBILITY OF THE TRAFFIC SIGNAL INDICATIONS, PRIOR TO CONSTRUCTION. ADDITIONAL CLEARANCE MAY BE REQUIRED AS DIRECTED BY THE UTILITY. THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY IF A CONFLICT IS DISCOVERED.
- PER CHAPTER 49.17 OF THE RCW AND TITLE 296 OF THE WAC, THE CONTRACTOR IS REQUIRED TO ENSURE SAFETY WHILE INSTALLING TRAFFIC SIGNAL SYSTEMS UNDER POWER LINES. THE CONTRACTOR SHALL COORDINATE WITH PSE TO DETERMINE INSTALLATION METHOD PRIOR TO WORK TAKING PLACE. IF LINES CAN NOT BE DE-ENERGIZED, A COMPANY WITH THE APPROPRIATE CERTIFICATION (SUCH AS POTELCO) IS REQUIRED TO STAND UP SIGNAL POLES AND PERFORM OTHER ELECTRICAL WORK UNDER THE DRIP LINE OF THE OVERHEAD UTILITY LINES. THE CONTRACTOR IS REQUIRED TO COORDINATE WITH THE CITY AND UTILITY TO ENSURE ALL APPLICABLE SAFETY STANDARDS ARE FOLLOWED.
- THE LOCATIONS OF FEATURES SHOWN ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION START.
- UTILITY LOCATIONS ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ANY SIGNAL WORK. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE TRUE ELEVATIONS AND LOCATIONS OF ALL UNDERGROUND UTILITIES AND THE EXTENT OF ANY HAZARD CREATED BY OVERHEAD UTILITY LINES. LOCATION, MARKING, AND RESPONSIBILITY FOR UNDERGROUND FACILITIES OR UTILITIES IS GOVERNED BY THE PROVISIONS OF CHAPTER 19.122 OF THE RCW. PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL CALL ONE-CALL (811) FOR UTILITY LOCATIONS.
- ALL WORK SHALL BE CONSISTENT WITH UTILITY AGENCY REQUIREMENTS. THE CONTRACTOR SHALL COORDINATE WITH AFFECTED UTILITY AGENCIES THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.
- JUNCTION BOX AND CONDUIT LOCATIONS ARE SHOWN FOR ILLUSTRATIVE PURPOSES. ACTUAL LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR IN THE FIELD AND VERIFIED BY THE ENGINEER/INSPECTOR PRIOR TO INSTALLATION. COINCIDENT CONDUITS MAY OCCUPY THE SAME TRENCH.
- NEW VEHICLE SIGNAL HEADS SHALL BE CONVENTIONAL TYPE WITH 12IN LED LENSES, TUNNEL VISORS, AND BACKPLATES. ALL VEHICLE SIGNAL HEADS SHALL BE FACTORY PAINTED FEDERAL GREEN. ALL BACK PLATES SHALL HAVE A TWO-INCH YELLOW REFLECTIVE STRIP AROUND ITS PERIMETER. ALL VEHICLE SIGNAL HEAD MOUNTS SHALL BE TYPE D OR TYPE F ON TYPE I SIGNAL STANDARDS, TYPE H OR TYPE K ON TYPE II AND TYPE III SIGNAL STANDARDS, AND TYPE M OR N ON MAST ARMS, WITH THE PLUMBIZER LOCATED BETWEEN THE RED AND YELLOW INDICATIONS, PER WSDOT STANDARD PLAN J-75.20, UNLESS OTHERWISE NOTED.
- NEW PEDESTRIAN SIGNAL HEADS SHALL BE COUNTDOWN TYPE LED WITH Z CRATE VISORS. HOUSING SHALL BE FACTORY PAINTED FEDERAL GREEN. ALL PEDESTRIAN SIGNAL HEAD MOUNTS SHALL BE TYPE C OR TYPE D ON TYPE I, II, AND III SIGNAL STANDARDS PER WSDOT STANDARD PLAN J-75.10, UNLESS OTHERWISE NOTED.
- NEW PEDESTRIAN PUSHBUTTONS SHALL BE APS TYPE.
- ALL NEW SPARE/EMPTY CONDUIT SHALL CONTAIN ELECTRICALLY DETECTABLE PULL TAPE AND BE MARKED AS "CITY OF FEDERAL WAY" CONDUIT.
- ALL NEW FOUNDATION LOCATIONS SHALL BE APPROVED BY THE ENGINEER. PRIOR TO EXCAVATION, THE CONTRACTOR SHALL CHECK FOR MINIMUM OVERHEAD CLEARANCES OF 16FT-6IN BETWEEN ALL SIGNAL HEADS/SIGNS/APPURTENANCES AND THE ROADWAY PRIOR TO POURING THE FOUNDATION.
- ALL JUNCTION BOXES SHALL BE INSTALLED PER WSDOT STANDARD PLANS J-40.10 AND J-40.30. JUNCTION BOXES LOCATED IN SIDEWALK SHALL HAVE NON-SLIP LIDS AND FRAMES.

- MAST ARM-MOUNTED SIGNS SHALL BE INSTALLED PER WSDOT STANDARD PLAN G-30.10.
- ALL NEW CABLES INSTALLED BY THE CONTRACTOR SHALL BE LABELED IN EACH JUNCTION BOX AND CABINET.
- EXISTING CONDUCTORS LISTED IN THE WIRING SCHEDULE ARE FOR REFERENCE ONLY. CONTRACTOR SHALL CONFIRM CONDUIT CONTENTS IN THE FIELD.
- ALL VEHICLE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS, AND PEDESTRIAN PUSHBUTTON ASSEMBLIES SHALL BE SECURELY COVERED WHILE NOT IN OPERATION.
- DIMENSIONS SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
- TRAFFIC SIGNAL CONDUCTOR TERMINATIONS SHALL BE MADE IN THE PRESENCE OF THE KING COUNTY LEAD TRAFFIC SIGNAL TECHNICIAN IN THE FIELD.
- MAINTAIN AND PROTECT ALL EXISTING TRAFFIC SIGNAL, ITS, AND STREET LIGHTING SYSTEMS AT ALL TIMES, UNLESS OTHERWISE NOTED. THE EXISTING TRAFFIC SIGNAL, ITS, AND STREET LIGHTING SYSTEMS SHALL REMAIN FULLY OPERATIONAL UNTIL THE NEW SYSTEM IS FULLY CONSTRUCTED AND READY FOR CHANGEOVER, PER THE SPECIAL PROVISIONS.
- A 3.5FT LEVEL WORK ZONE SHALL BE PROVIDED ON ALL SIDES OF NEW CABINET INSTALLATIONS.
- PROPOSED AND MODIFIED JUNCTION BOXES SHALL BE BONDED AND GROUNDED PER NEC REQUIREMENTS.
- FOUNDATIONS SHALL BE REMOVED IN ACCORDANCE WITH SECTION 2-02.3(1) OF THE STANDARD SPECIFICATIONS.
- REFER TO SHEETS 67 AND 68 FOR ADDITIONAL DETAILS.
- REFER TO WSDOT STANDARD PLAN J-75.30 FOR ADDITIONAL TRAFFIC SIGNAL DETAILS.
- THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF FEDERAL WAY'S FIBER OPTIC LOOP PROJECT (CITY PROJECT NO. 36239), WHICH WILL BE INSTALLING NEW FIBER OPTIC COMMUNICATIONS ALONG THE SOUTH SIDE OF S 288TH ST.

DETECTION NOTES

- ALL DETECTION LOOPS/ZONES SHALL BE LOCATED IN THE CENTER OF THE TRAVEL LANE.
- FILTER DETECTION LOOPS/ZONES SHALL BE LOCATED 4' ON-CENTER BACK FROM THE STOP LINE, UNLESS OTHERWISE NOTED.
- NOT ALL EXISTING DETECTION LOOPS/ZONES SHOWN. ALL EXISTING DETECTION LOOPS/ZONES SHALL BE MAINTAINED, UNLESS OTHERWISE NOTED.
- ADVANCE LEFT-TURN LANE DETECTION LOOPS/ZONES SHALL BE LOCATED AT THE BEGINNING OF THE LEFT-TURN LANE, UNLESS OTHERWISE NOTED.
- ADVANCE THRU-LANE DETECTION LOOPS/ZONES SHALL BE LOCATED BASED ON THE FOLLOWING ON-CENTER DISTANCES BACK FROM THE STOP LINE, BASED ON THE APPROACH'S POSTED SPEED LIMIT:
 20 MPH: 210'
 25 MPH: 260'
 30 MPH: 310'
 35 MPH: 360'
 40 MPH: 415'
 45 MPH: 465'

LEGEND

DESCRIPTION	EXISTING SYMBOL	PROPOSED SYMBOL
RIGHT-OF-WAY		
TEMPORARY CONSTRUCTION EASEMENT		
CONDUIT		
CONSTRUCTION NOTE		
SIGNAL POLE CONSTRUCTION NOTE		
WIRE NOTE		
SIGN NOTE		
SIGNAL POLE AND MAST ARM		
TYPE PS/TYP 1 SIGNAL STANDARD		
PEDESTRIAN PUSHBUTTON POST		
VEHICLE SIGNAL HEAD		
VEHICLE SIGNAL TURN HEAD		
PEDESTRIAN SIGNAL HEAD		
PEDESTRIAN PUSHBUTTON		
TENON		
EVP DETECTOR		
INDICATION LIGHT		
WIRELESS ANTENNA		
MAST ARM SIGN		
TRAFFIC SIGNAL CONTROLLER CABINET		
ELECTRICAL SERVICE CABINET		
MAIN BATTERY BACKUP SYSTEM CABINET		
AUXILIARY BATTERY BACKUP SYSTEM CABINET		
JUNCTION BOX, TYPE 1		
JUNCTION BOX, TYPE 2		
JUNCTION BOX, TYPE 8		
25-TA VAULT		
HYBRID RADAR/VIDEO DETECTION CAMERA		
VIDEO DETECTION CAMERA		
CCTV CAMERA		
RADAR/VIDEO DETECTION ZONE		
FILTER DETECTION ZONE		
EXIT DETECTION ZONE		
LUMINAIRE		

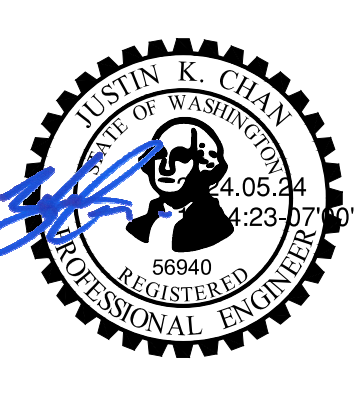


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TRAFFIC SIGNAL NOTES AND LEGEND

CITY PROJECT #:
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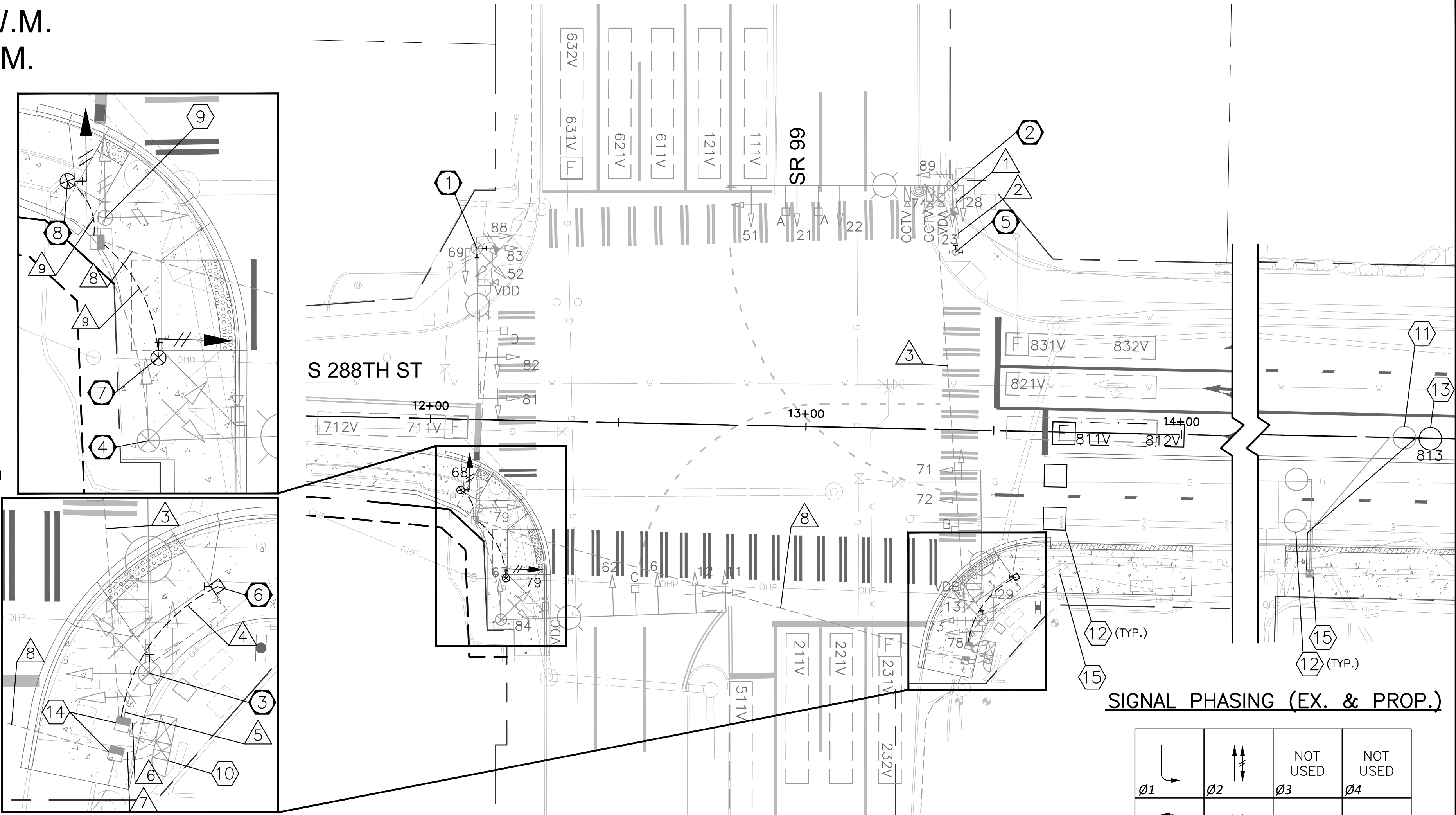
NOTES

- SEE SHEET 53 FOR GENERAL NOTES AND LEGEND.
- SEE SHEET 55 FOR TRAFFIC SIGNAL WIRING DIAGRAM.
- SEE SHEETS 56 FOR TRAFFIC SIGNAL POLE CHART.
- NOT ALL EXISTING DETECTION SHOWN.

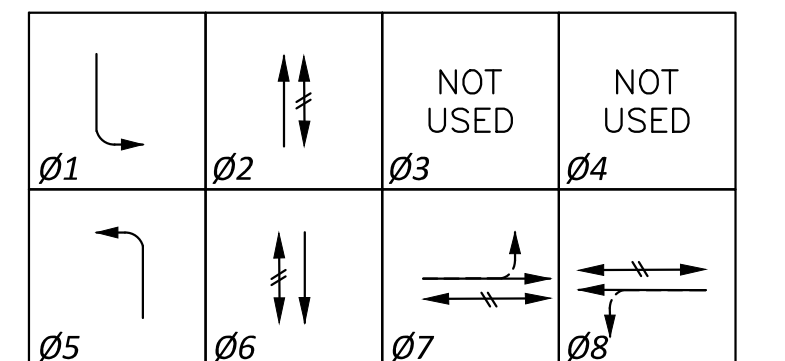
**SEC.32/33 T.22N. R.4E. W.M.
SEC.4/5 T.21N. R.4E. W.M.**

CONSTRUCTION NOTES (THIS SHEET ONLY)

- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. REMOVE TWO EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLIES AND REPLACE WITH TWO APS PEDESTRIAN PUSHBUTTON ASSEMBLIES WITH AUDIBLE SPEECH MESSAGES ON EXISTING POLE. USE EXISTING CONDUCTORS. INSTALL INTERFACE UNITS IN EXISTING COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ROUTE CONDUCTORS TO APS PEDESTRIAN PUSHBUTTON ASSEMBLIES PER MANUFACTURER'S RECOMMENDATIONS.
- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. INSTALL INTERFACE UNITS IN EXISTING COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ROUTE CONDUCTORS TO APS PEDESTRIAN PUSHBUTTON ASSEMBLIES PER MANUFACTURER'S RECOMMENDATIONS.
- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. REMOVE TWO EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLIES. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY WITH AUDIBLE SPEECH MESSAGE ON EXISTING POLE. USE EXISTING CONDUCTORS. INSTALL INTERFACE UNITS IN EXISTING COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ROUTE CONDUCTORS TO APS PEDESTRIAN PUSHBUTTON ASSEMBLIES PER MANUFACTURER'S RECOMMENDATIONS. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT.
- MAINTAIN AND PROTECT EXISTING TYPE PPB POST, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. REMOVE TWO EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLIES. INSTALL TWO APS PEDESTRIAN PUSHBUTTON ASSEMBLIES WITH ADAPTERS AND AUDIBLE SPEECH MESSAGES ON EXISTING POST. USE EXISTING CONDUCTORS. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- INSTALL TYPE PPB POST AND FOUNDATION PER WSDOT STANDARD PLANS J-20.10 AND J-20.26. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY WITH AUDIBLE SPEECH MESSAGE ON POST. TOP OF FOUNDATION SHALL BE FLUSH WITH FINISHED SIDEWALK.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PEDESTRIAN PUSHBUTTON WITH AUDIBLE SPEECH MESSAGE AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PEDESTRIAN PUSHBUTTON WITH AUDIBLE SPEECH MESSAGE AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK.
- REMOVE EXISTING POLE AND ALL ASSOCIATED EQUIPMENT AND SALVAGE TO THE KING COUNTY SIGNAL SHOP. REMOVE EXISTING FOUNDATION, CONDUIT UPSWEEPS, AND ASSOCIATED WIRING. BACKFILL AND COMPACT VOIDS.
- MAINTAIN AND PROTECT EXISTING TRAFFIC SIGNAL CONTROLLER, ELECTRICAL SERVICE, AND KING COUNTY METRO TSP CABINETS. ADJUST EXISTING VIDEO DETECTION ZONES TO ACCOMMODATE PROPOSED CHANNELIZATION, AS NEEDED.
- ABANDON EXISTING INDUCTION LOOP DETECTOR. REMOVE EXISTING LOOP LEAD-IN CABLES BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET.
- REFER TO SHEET 57 FOR DETAILS.
- INSTALL TYPE 3 INDUCTION LOOP PER WSDOT STANDARD PLANS J-50.05, J-50.12, AND J-50.15. SPLICE TO EXISTING LOOP LEAD-IN CABLE IN EXISTING JUNCTION BOX.
- REPLACE EXISTING JUNCTION BOX WITH TYPE 8 JUNCTION BOX WITH NON-SLIP LID AND FRAME PER WSDOT STANDARD PLAN J-40.30. ADJUST TO MATCH FINISHED GRADE.
- REPLACE EXISTING JUNCTION BOX WITH TYPE 1 JUNCTION BOX WITH NON-SLIP LID AND FRAME PER WSDOT STANDARD PLAN J-40.10. ADJUST TO MATCH FINISHED GRADE.



SIGNAL PHASING (EX. & PROP.)

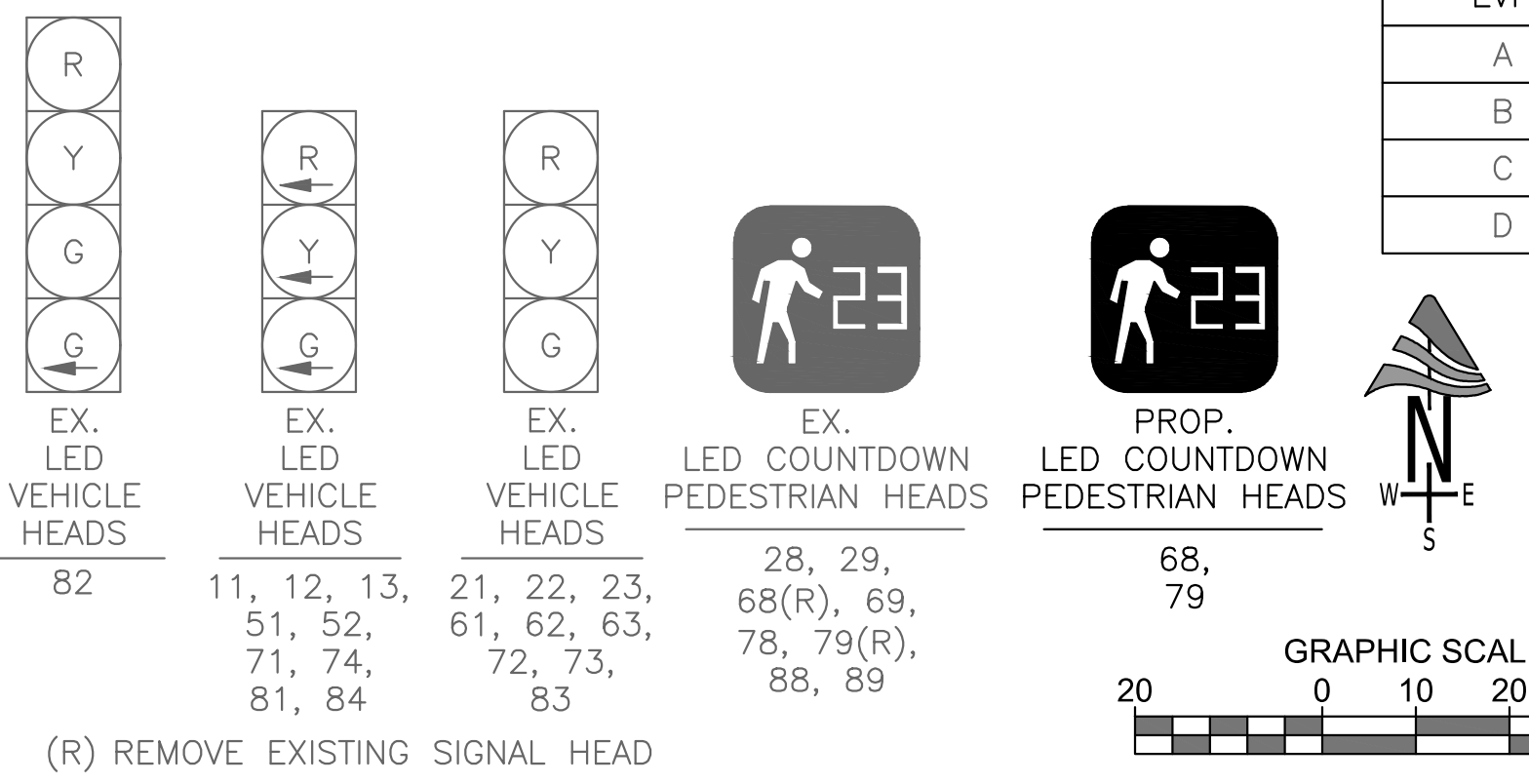


- PROTECTED MOVEMENT
- - - PERMITTED MOVEMENT
- ↔ PEDESTRIAN MOVEMENT

WIRING SCHEDULE (THIS SHEET ONLY)

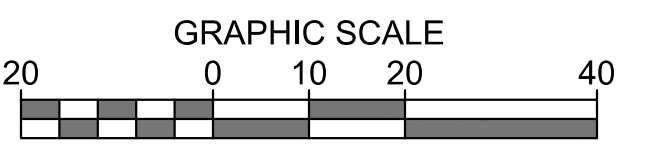
NO	RACEWAY CONDUIT SIZE	LOOP 2C(SH)		PPB 2C(SH)		EVP IND. 2C		EVP DETECT 3C(SH)		VIDEO DETECT		PED HEAD COMM 4C		PED HEAD 5C		VEH HEAD 5C		CCTV CAT6		ILLUM #8		CONDUIT FILL %
		EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	
1	EX 3"					1		2		1		2		2		3		2		2		13.3%
	EX 1.5"											2								2		EX
2	EX 2"			2								2										12.6%
	EX 3"			2				2		1												EX
3	EX 3"					1						2			4							EX
	EX 1.5"																			2		EX
4	2" SCH40				1							1										6.4%
	EX 3"			1, 1(R)		1		1		1		1		2		2						15.3%
	EX 1.5"																			2		EX
5	EX 3"			3, 1(R)		1		3		2									2			13.2%
	EX 3"					2		2		2					4		6					29.7%
6	EX 3"	3		2, 2(R)	2			2		2				2, 2(R)	2	6						14.6%
	EX 2"																					14.6%
SPARE (EMPTY WITH PULL TAPE)																						
	EX 4"					2						2, 2(R)	2	6								13.0%
	EX 3"	3		2, 2(R)	2			2		2										4		7.9%
	EX 1.5"																					EX
9	2" SCH40				1									1								6.1%

SIGNAL HEAD ASSIGNMENT (THIS SHEET ONLY)



PRE-EMPTION SCHEDULE (EX.)

EVP	PHASE(S)
A	Ø2, Ø5
B	Ø7
C	Ø1, Ø6
D	Ø8



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KIRKLAND, WASHINGTON 98034
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(FAX) 425 825-8434

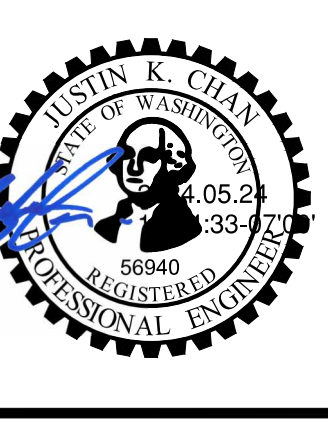
* ALL CONDUIT SHALL CONTAIN A GROUND WIRE EQUAL IN SIZE TO THE LARGEST CONDUCTOR (NO. 8 MINIMUM), UNLESS OTHERWISE NOTED. IF ONE DOES NOT EXIST, INSTALL A NEW GROUND WIRE.
(R) REMOVE EXISTING CONDUCTOR(S).

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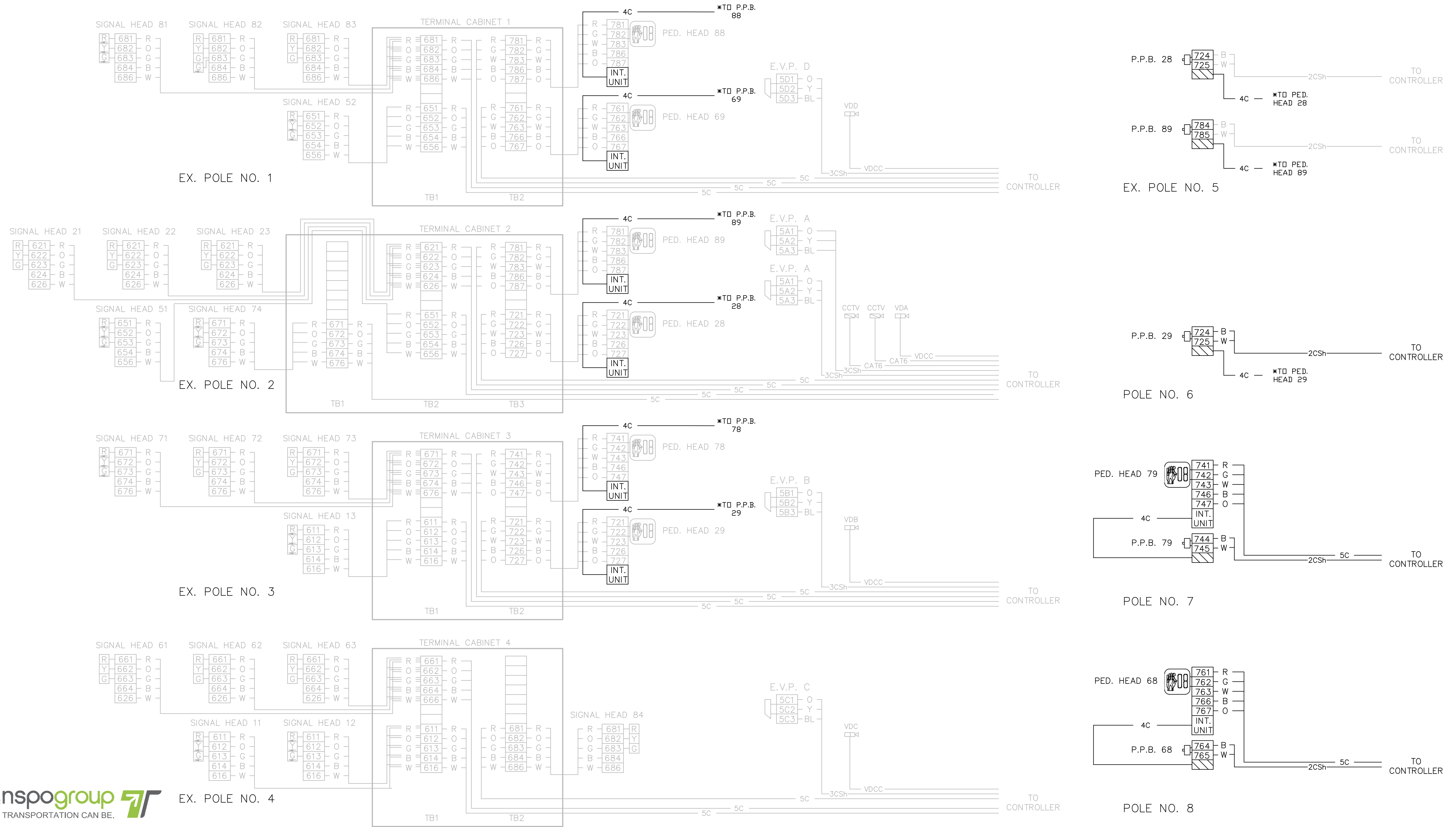
DRAWING VERSION / REVISION LOG

NO.	DATE	REVISION

S 288TH ST ROAD DIET - PHASE 2
TRAFFIC SIGNAL PLAN
SR 99 & S 288TH ST

CITY PROJECT #:
36238
SHT. **54**
OF **81**

FIELD WIRE TERMINATIONS

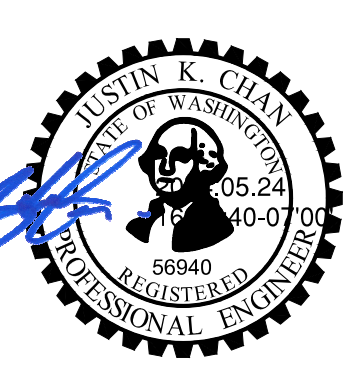


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NO.	DATE	REVISION

S 288TH ST ROAD DIET - PHASE 2

TRAFFIC SIGNAL WIRING DIAGRAM

SR 99 & S 288TH ST

CITY PROJECT #:
36238

SHT. **55**
OF **81**

LEGEND

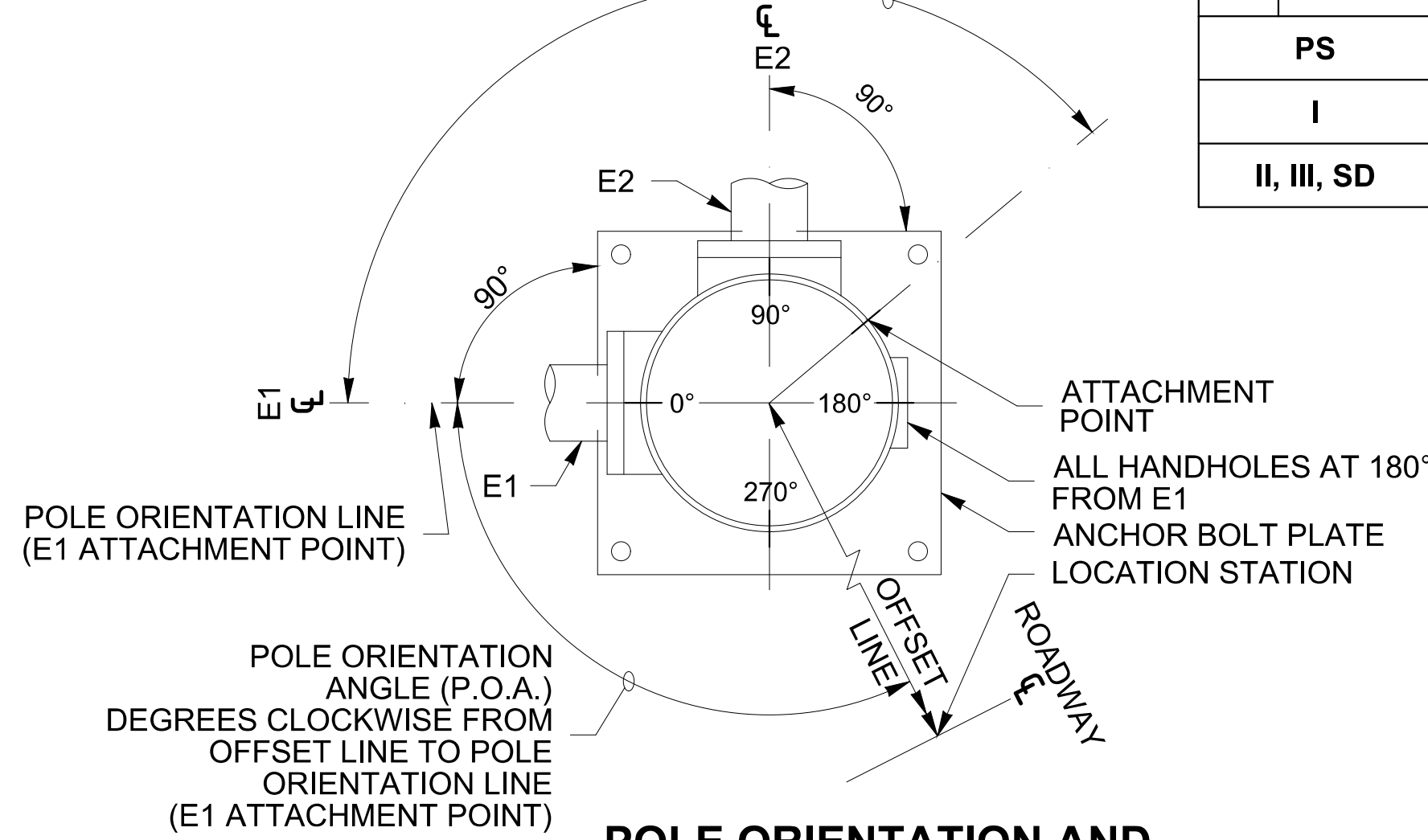
- a. VEHICLE DISPLAY
g. PEDESTRIAN DISPLAY
b. MASTARM MTD. SIGN
h. TERMINAL CABINET
c. STREET NAME SIGN
i. APS PPB-M
d. PRE-EMPT DETECTOR
j. HANDHOLE
e. POST MTD. SIGN
k. DETECTION CAMERA
f. LUMINAIRE

NOTES

- 1 MOUNTING COUPLING INSTALLED AT OFFSET DISTANCE INDICATED IN CHART. FOR TYPE N MOUNTS ONLY, DRILL 1" DIA. HOLE IN MASTARM AND INSTALL PLASTIC SPLIT BUSHING FOR CABLE ENTRANCE.
2 FIELD INSTALLED. SIGN SIZES SHALL NOT EXCEED THE MAXIMUM VALUES LISTED HERE:

Table with 4 columns: SIGN, HEIGHT, WIDTH, AREA. Rows b, c, e with corresponding dimensions.

MEASURE ATTACHMENT POINT ANGLES CLOCKWISE FROM POLE ORIENTATION LINE (E1 ATTACHMENT POINT) TO THE ATTACHMENT POINT LOCATION

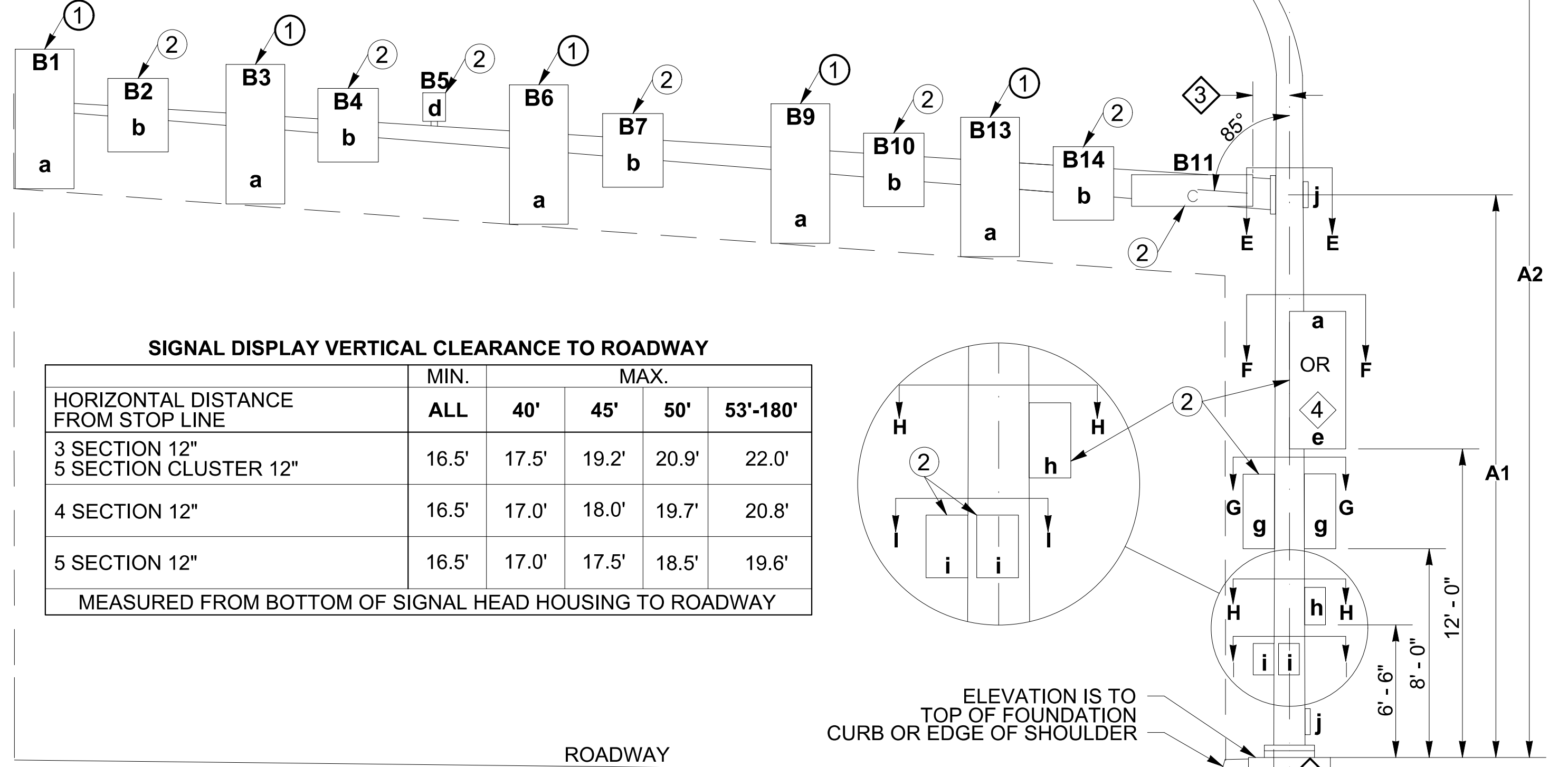
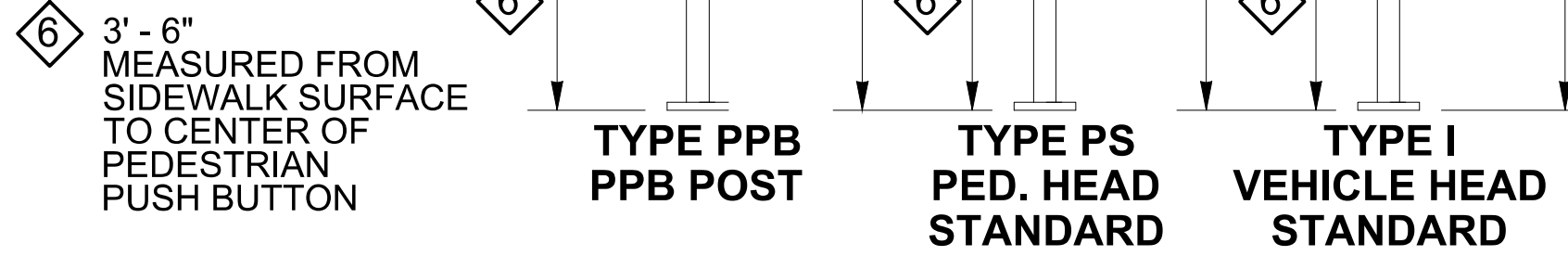


POLE ORIENTATION AND ATTACHMENT POINT DETAIL

NOTE: TYPE E MOUNTS SHALL BE USED FOR PEDESTRIAN DISPLAYS ON TYPE II OR III SIGNAL STANDARDS. WITH THE FOLLOWING EXCEPTION: PEDESTRIAN DISPLAYS MOUNTED ON OCTAGONAL (8 SIDED) SIGNAL STANDARDS AT AN ANGLE OTHER THAN A 45 DEGREE INCREMENT SHALL USE A TYPE A MOUNT FOR TWO PEDESTRIAN DISPLAYS, OR A TYPE B MOUNT FOR A SINGLE PEDESTRIAN DISPLAY.

Table: STANDARD TYPE vs STANDARD PLAN REFERENCES (POLE, FOUNDATION, ELECTRICAL).

Table: LUMINAIRE MASTARM XYZ (ft³) vs mastarm LENGTH.



- 3 1'-0" MIN. TO 2'-6" MAX. FROM POLE CENTERLINE TO SIGN EDGE
4 FOR POST MOUNTED SIGNS THERE SHALL BE 2'-0" MIN. FROM THE FACE OF THE CURB OR THE EDGE OF THE SHOULDER TO THE EDGE OF THE SIGN
5 PLACEMENT SHALL BE 2'-0" MIN. FROM FACE OF CURB OR EDGE OF SHOULDER; 3'-0" MIN. FROM FACE OF GUARDRAIL; 4'-0" MIN. FROM CONC. BARRIER TYPE 2
6 3'-6" MEASURED FROM SIDEWALK SURFACE TO CENTER OF PEDESTRIAN PUSH BUTTON
7 DMS OR VMS IS NOT ALLOWED ON THESE POLES.
8 MOUNT PER MANUFACTURER'S RECOMMENDATION

SIGNAL STANDARD DETAIL CHART

Table: SIGNAL STANDARD DETAIL CHART with columns for STD. No., CENTERLINE STATIONING, FIELD LOCATION, MOUNTING HEIGHT, SIGNAL MASTARM DATA, WINDLOAD AREAS, LUMINAIRE ARM, FOUNDATION DESIGN, etc.

EX = EXISTING
EX(R) = EXISTING TO BE REMOVED
* CALCULATED POLE XYZ (FT³) IS THE SUM OF THE TOTAL XYZ (FT³) FOR THE SIGNAL MAST ARM AND THE XYZ (FT³) FOR THE LUMINAIRE ARM (IF PRESENT).
** TOP OF FOUNDATION SHALL BE FLUSH WITH THE ADJACENT FINISHED GRADE (E.G., SIDEWALK, CURB, ETC.).
*** ASSUMED SOIL BEARING PRESSURE OF 2,500 PSF AND DRILLED SHAFT CONSTRUCTION, BASED ON THE DRAFT GEOTECHNICAL ENGINEERING REPORT PROVIDED BY WOOD ENVIRONMENT & INFRASTRUCTURE SOLUTIONS, INC. ON AUGUST 5, 2022.



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Table: DRAWING VERSION / REVISION LOG with columns: NO., DATE, REVISION

S 288TH ST ROAD DIET - PHASE 2
TRAFFIC SIGNAL POLE CHART
SR 99 & S 288TH ST

CITY PROJECT #: 36238
SHT. 56 OF 81

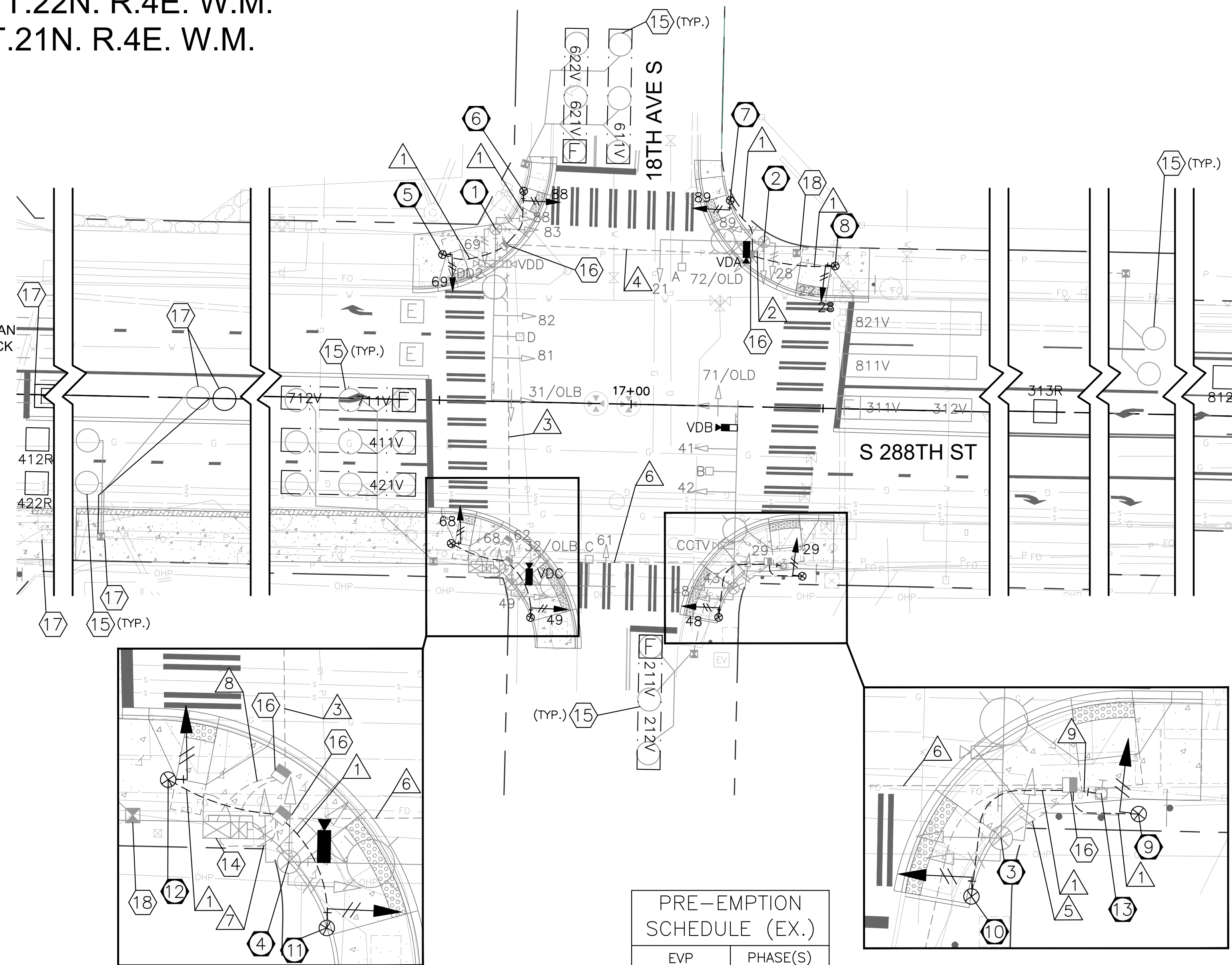
SEC.32/33 T.22N. R.4E. W.M.
SEC.4/5 T.21N. R.4E. W.M.

NOTES

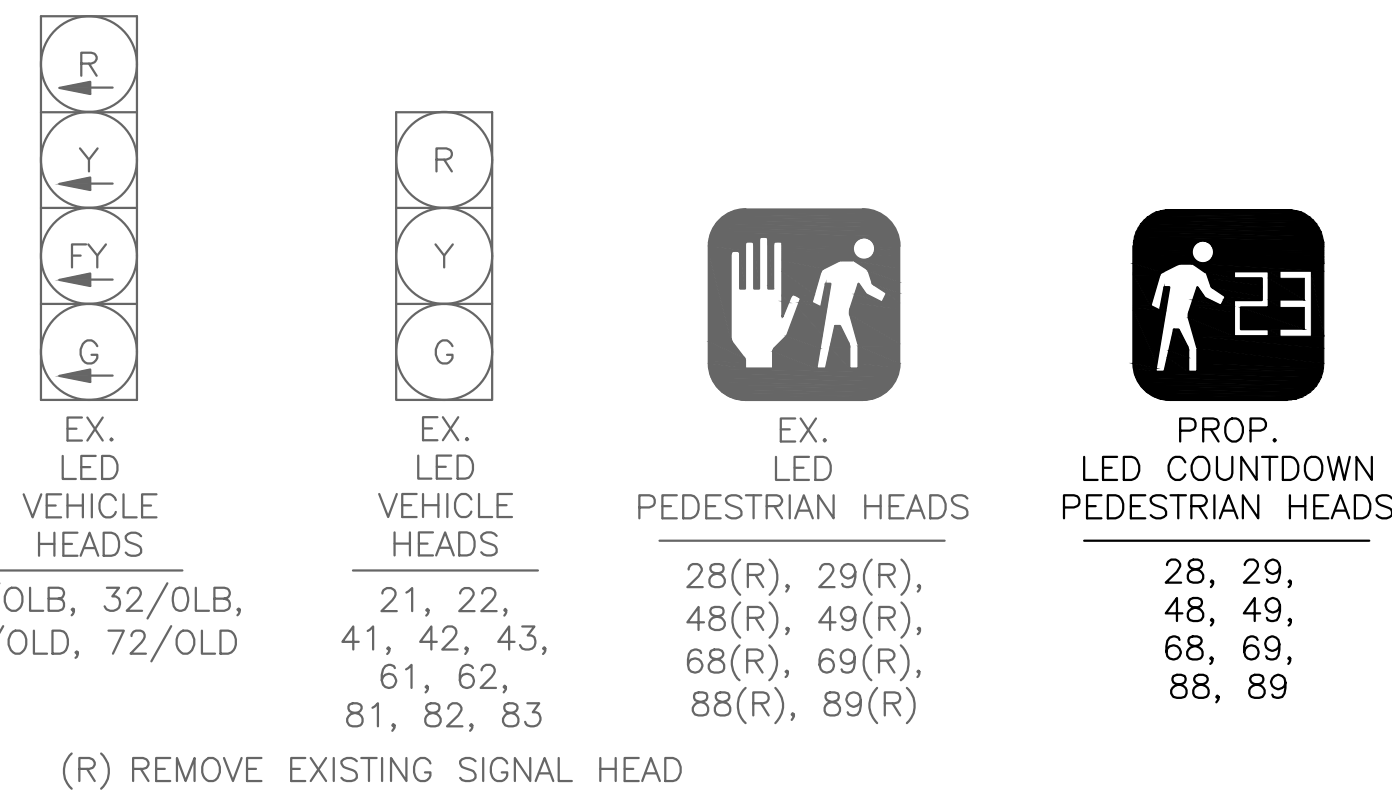
- SEE SHEET 53 FOR GENERAL NOTES AND LEGEND.
- SEE SHEET 58 FOR TRAFFIC SIGNAL WIRING SCHEDULE.
- SEE SHEET 59 FOR TRAFFIC SIGNAL WIRING DIAGRAM.
- SEE SHEETS 60 FOR TRAFFIC SIGNAL POLE CHART.

CONSTRUCTION NOTES (THIS SHEET ONLY)

- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. REMOVE ONE EXISTING R10-12 (MOD) SIGN ON MAST ARM. REMOVE TWO EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLIES AND TWO EXISTING PEDESTRIAN SIGNAL HEADS. REMOVE EXISTING PEDESTRIAN PUSHBUTTON AND PEDESTRIAN SIGNAL HEAD CONDUCTORS BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. INSTALL ONE VIDEO DETECTION CAMERA ON EXISTING LUMINAIRE ARM PER MANUFACTURER'S RECOMMENDATIONS. REMOVE TWO EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLIES AND TWO EXISTING PEDESTRIAN SIGNAL HEADS. REMOVE EXISTING PEDESTRIAN PUSHBUTTON AND PEDESTRIAN SIGNAL HEAD CONDUCTORS BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. INSTALL ONE HYBRID RADAR/VIDEO DETECTION CAMERA ON EXISTING MAST ARM PER MANUFACTURER'S RECOMMENDATIONS. REMOVE ONE EXISTING R10-12 (MOD) SIGN ON MAST ARM. REMOVE ONE EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLY AND TWO EXISTING PEDESTRIAN SIGNAL HEADS. REMOVE EXISTING PEDESTRIAN PUSHBUTTON AND PEDESTRIAN SIGNAL HEAD CONDUCTORS BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- MAINTAIN AND PROTECT EXISTING TYPE III SIGNAL STANDARD, MAST ARM, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. INSTALL ONE VIDEO DETECTION CAMERA ON EXISTING LUMINAIRE ARM PER MANUFACTURER'S RECOMMENDATIONS. REMOVE TWO EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLIES AND TWO EXISTING PEDESTRIAN SIGNAL HEADS. REMOVE EXISTING PEDESTRIAN PUSHBUTTON AND PEDESTRIAN SIGNAL HEAD CONDUCTORS BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11, J-20.26, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11, J-20.26, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11, J-20.26, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11, J-20.26, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHBUTTON ASSEMBLY PER MANUFACTURER'S RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- MAINTAIN AND PROTECT EXISTING TYPE PPB POST, FOUNDATION, AND ALL ASSOCIATED EQUIPMENT. REMOVE ONE EXISTING PEDESTRIAN PUSHBUTTON ASSEMBLY. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY ON EXISTING POST. PLUG AND SEAL HOLES PER DETAIL ON SHEET 67.
- MAINTAIN AND PROTECT EXISTING TRAFFIC SIGNAL CONTROLLER AND ELECTRICAL SERVICE CABINETS. ADJUST EXISTING VIDEO DETECTION ZONES TO ACCOMMODATE PROPOSED CHANNELIZATION, AS NEEDED.
- ABANDON EXISTING INDUCTION LOOP DETECTOR. REMOVE EXISTING LOOP LEAD-IN CABLES BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET.
- REPLACE EXISTING JUNCTION BOX WITH TYPE 8 JUNCTION BOX WITH NON-SLIP LID AND FRAME PER WSDOT STANDARD PLAN J-40.30. ADJUST TO MATCH FINISHED GRADE.
- REFER TO SHEET 54 FOR DETAILS.
- REPLACE EXISTING JUNCTION BOX WITH TYPE 1 JUNCTION BOX WITH NON-SLIP LID AND FRAME PER WSDOT STANDARD PLAN J-40.10. ADJUST TO MATCH FINISHED GRADE.

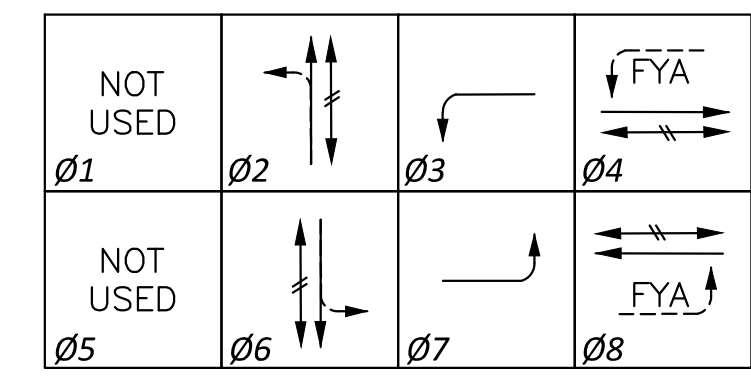


SIGNAL HEAD ASSIGNMENT (THIS SHEET ONLY)

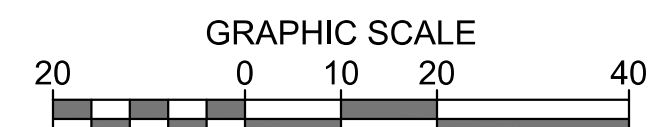


PRE-EMPTION SCHEDULE (EX.)	
EVP	PHASE(S)
A	Ø2
B	Ø4, Ø7
C	Ø6
D	Ø3, Ø8

SIGNAL PHASING (EX. & PROP.)



→ PROTECTED MOVEMENT
- - - PERMITTED MOVEMENT
- - - PEDESTRIAN MOVEMENT



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TRAFFIC SIGNAL PLAN
18TH AVE S & S 288TH ST

CITY PROJECT #: 36238
SHT. 57 OF 81

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WIRING SCHEDULE (PREVIOUS SHEET ONLY)																									
NO	RACEWAY CONDUIT SIZE	LOOP 2C(SH)		PPB 2C(SH)		EVP IND. 2C		EVP DETECT 3C(SH)		VIDEO DETECT		PED HEAD COMM 4C		PED HEAD 5C		VEH HEAD 5C		CCTV CAT6		INTER-CONNECT 6PR		ILLUM #8		CONDUIT FILL %	
		EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP		
1	2" SCH40				1								1		1										10.7%
2	EX 3"			2(R)		1		1		1			2(R)		2										8.6%
	EX 1.5"																					2			EX
3	EX 3"	6(R)		4(R)	4			2		2	1														11.3%
	EX 3"					2							4(R)	4	4										21.1%
	EX 1.5"																					2			EX
4	EX 3"	2(R)		2(R)	2			1		1															5.7%
	EX 3"					1							2(R)	2	2										11.1%
5	EX 1.5"																						2		EX
	EX 3"			2(R)		1		1		1			2(R)		3		1								11.7%
6	EX 3"	2(R)		2(R)	2			1		1								1		1					11.5%
	EX 2"					1							2(R)	2	2										24.8%
	EX 1.5"																					2			EX
7	EX 3"	2(R)		4(R)	4			2		2								1		1					16.3%
	EX 3"					2							4(R)	4	4										21.1%
	EX 1.5"																					2			EX
8	EX 3"	6(R)		4(R)	4			2		2	1														11.3%
	EX 3"					2							4(R)	4	4										21.1%
9	EX 1.5"			1(R)	1								1												12.4%




* ALL CONDUIT SHALL CONTAIN A GROUND WIRE EQUAL IN SIZE TO THE LARGEST CONDUCTOR (NO. 8 MINIMUM), UNLESS OTHERWISE NOTED. IF ONE DOES NOT EXIST, INSTALL A NEW GROUND WIRE.
(R) REMOVE EXISTING CONDUCTOR(S).

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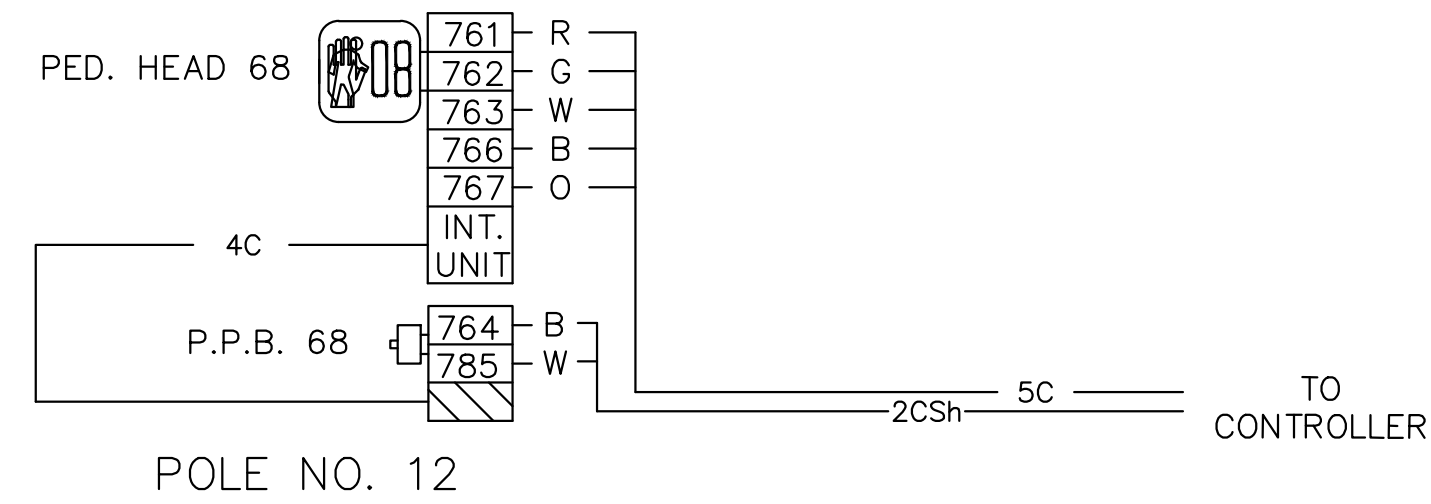
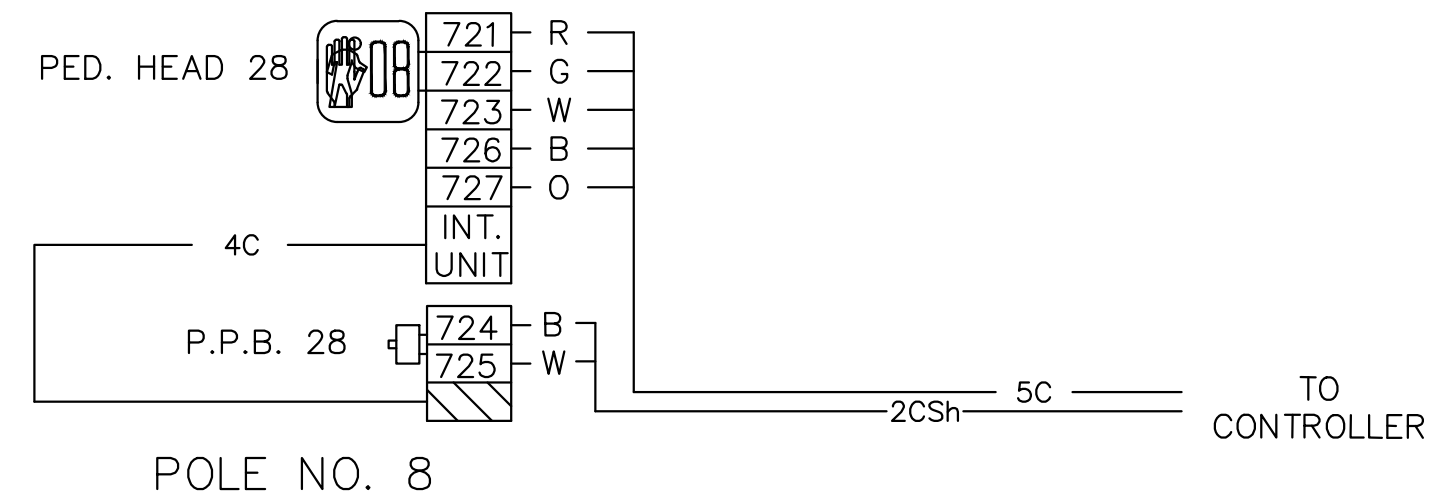
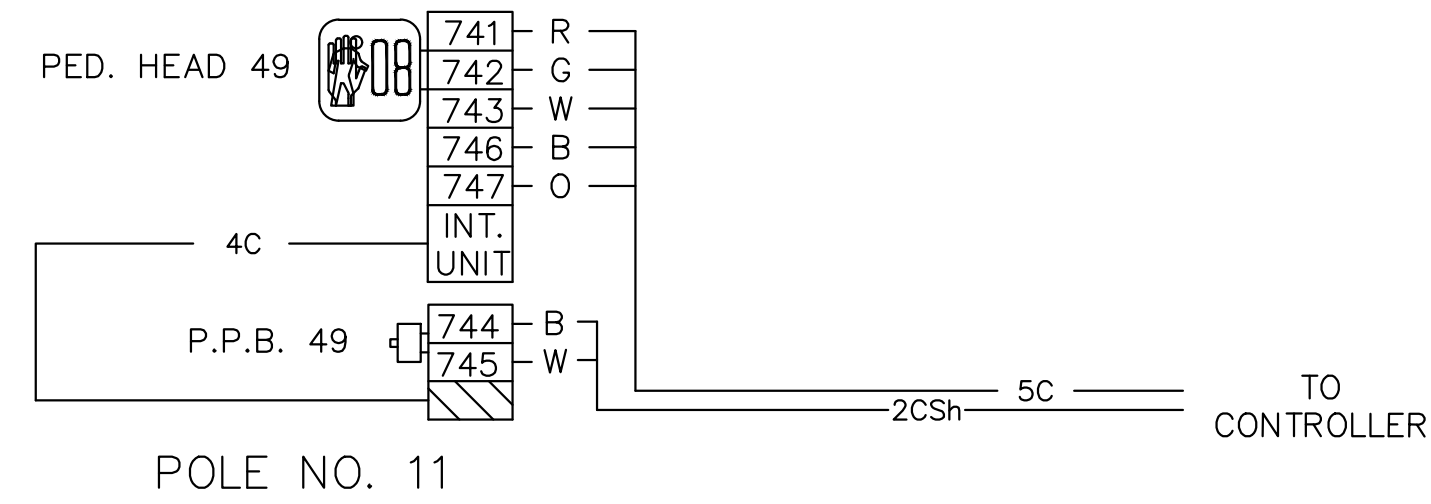
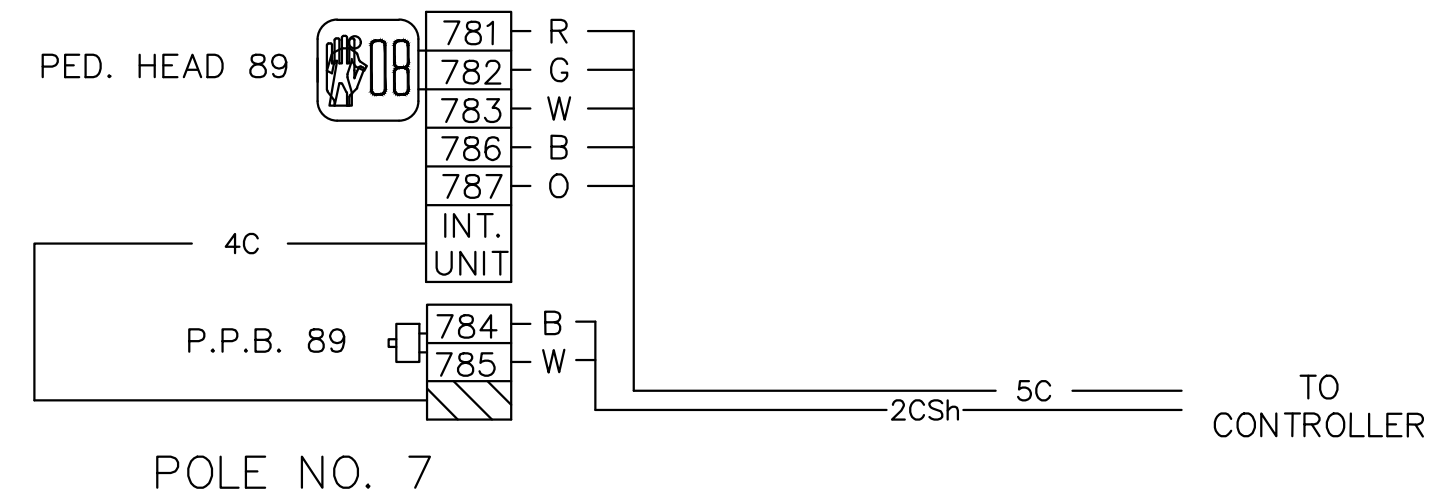
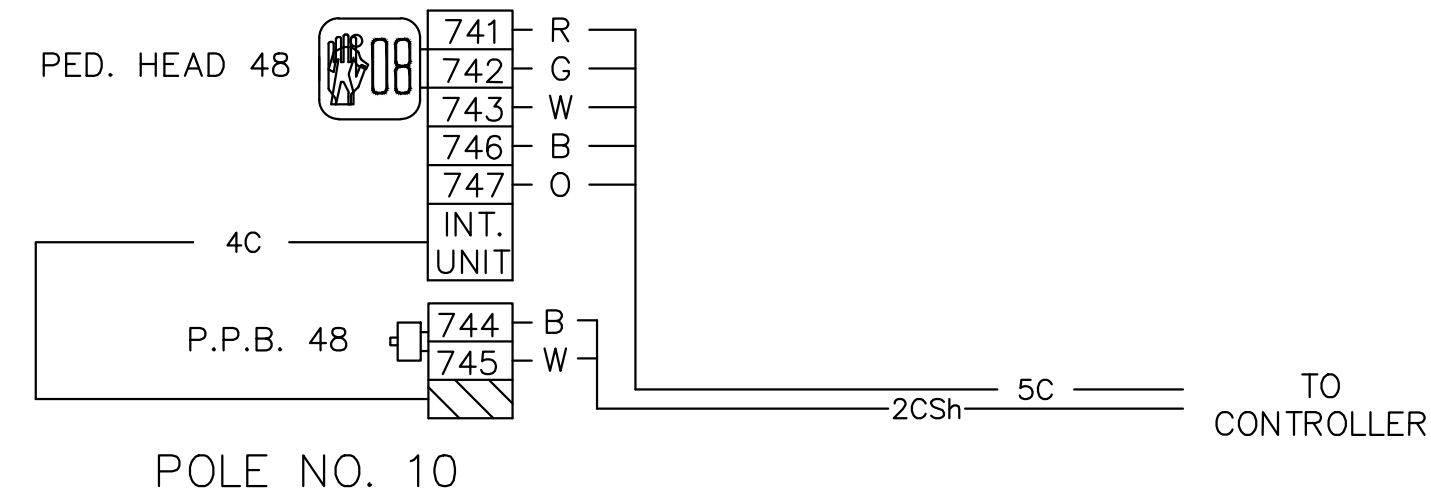
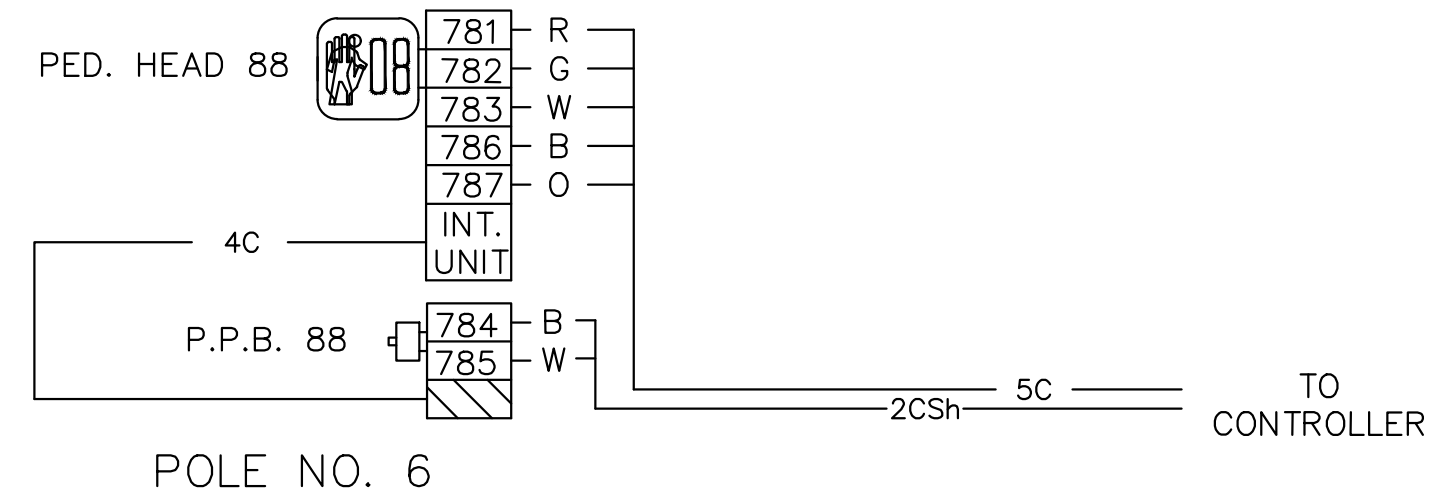
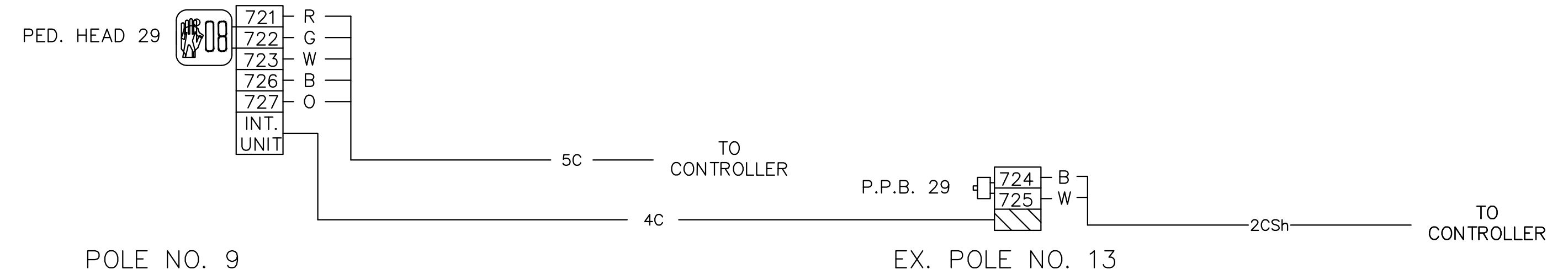
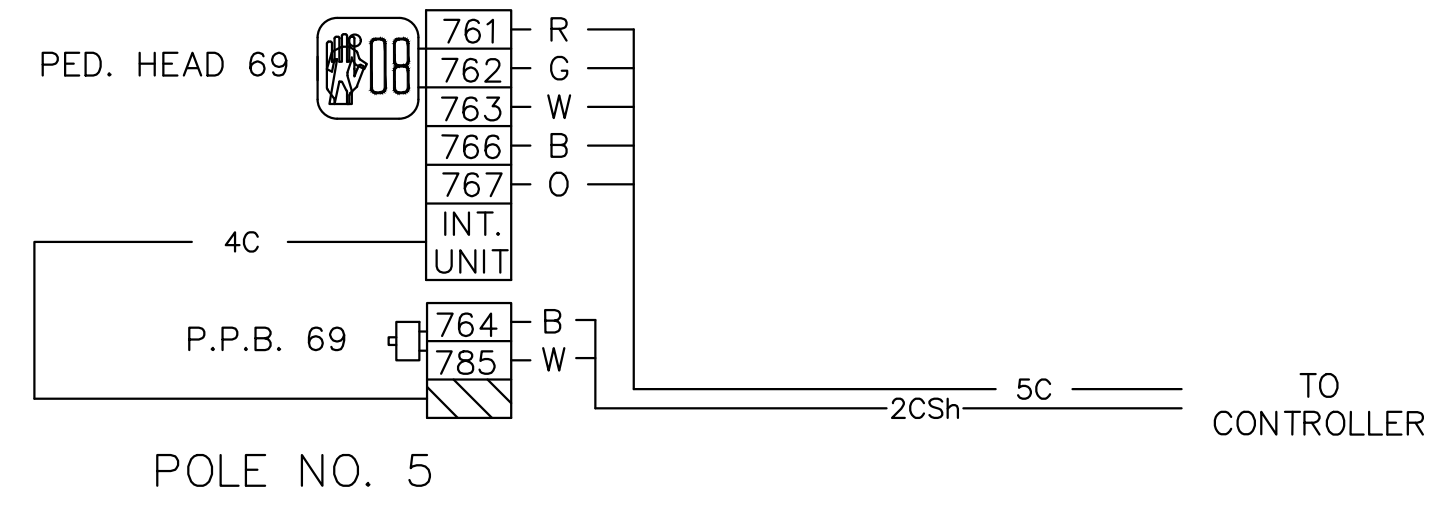


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				NO.	DATE	REVISION		

FIELD WIRE TERMINATIONS



NOTES

1. NOT ALL EXISTING POLES SHOWN.



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APPROVED: RWP



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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC SIGNAL WIRING DIAGRAM
18TH AVE S & S 288TH ST

CITY PROJECT #:
36238

SHT. **59**
OF **81**

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CONSTRUCTION NOTES (SHEET 63 ONLY)


- 1 INSTALL TYPE III SIGNAL STANDARD WITH 55FT MAST ARM AND FOUNDATION PER WSDOT STANDARD PLAN J-26.10. INSTALL THREE VEHICLE SIGNAL HEADS, TWO ADDITIONAL TENONS FOR FUTURE VEHICLE SIGNAL HEADS, ONE EVP DETECTOR WITH INDICATOR LIGHT, ONE REGULATORY SIGN, AND ONE STREET NAME SIGN ON MAST ARM. INSTALL ONE TERMINAL CABINET, ONE VEHICLE SIGNAL HEAD, AND ONE RELOCATED WIRELESS ANTENNA ON POLE. COORDINATE INSTALLATION OF WIRELESS ANTENNA WITH FEDERAL WAY IT. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB. INSTALL 123W ARCHAION LARGE LED LUMINAIRE FIXTURE (ARCH-L-PA3-120-740-U-T3-AP-10MSP-PR7) WITH CIMCON NODE AT 35FT MOUNTING HEIGHT WITH 16FT LUMINAIRE ARM. INSTALL ONE HYBRID RADAR/VIDEO DETECTION CAMERA AND TWO CCTV CAMERAS ON LUMINAIRE ARM PER MANUFACTURERS' RECOMMENDATIONS.
- 2 INSTALL TYPE III SIGNAL STANDARD WITH 45FT MAST ARM AND FOUNDATION PER WSDOT STANDARD PLAN J-26.10. INSTALL THREE VEHICLE SIGNAL HEADS, ONE EVP DETECTOR WITH INDICATOR LIGHT, ONE REGULATORY SIGN, AND ONE STREET NAME SIGN ON MAST ARM. INSTALL ONE TERMINAL CABINET AND ONE VEHICLE SIGNAL HEAD ON POLE. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB. INSTALL 123W ARCHAION LARGE LED LUMINAIRE FIXTURE (ARCH-L-PA3-120-740-U-T3-AP-10MSP-PR7) WITH CIMCON NODE AT 35FT MOUNTING HEIGHT WITH 16FT LUMINAIRE ARM. INSTALL ONE HYBRID RADAR/VIDEO DETECTION CAMERA ON LUMINAIRE ARM PER MANUFACTURERS' RECOMMENDATIONS.
- 3 INSTALL TYPE III SIGNAL STANDARD WITH 40FT MAST ARM AND FOUNDATION PER WSDOT STANDARD PLAN J-26.10. INSTALL TWO VEHICLE SIGNAL HEADS, TWO ADDITIONAL TENONS FOR FUTURE VEHICLE SIGNAL HEADS, ONE EVP DETECTOR WITH INDICATOR LIGHT, AND ONE STREET NAME SIGN ON MAST ARM. INSTALL ONE TERMINAL CABINET AND ONE VEHICLE SIGNAL HEAD ON POLE. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB. INSTALL 123W ARCHAION LARGE LED LUMINAIRE FIXTURE (ARCH-L-PA3-120-740-U-T3-AP-10MSP-PR7) WITH CIMCON NODE AT 35FT MOUNTING HEIGHT WITH 12FT LUMINAIRE ARM. INSTALL ONE HYBRID RADAR/VIDEO DETECTION CAMERA ON LUMINAIRE ARM PER MANUFACTURERS' RECOMMENDATIONS.
- 4 INSTALL TYPE III SIGNAL STANDARD WITH 40FT MAST ARM AND FOUNDATION PER WSDOT STANDARD PLAN J-26.10. INSTALL THREE VEHICLE SIGNAL HEADS, ONE EVP DETECTOR WITH INDICATOR LIGHT, TWO REGULATORY SIGNS, AND ONE STREET NAME SIGN ON MAST ARM. INSTALL ONE TERMINAL CABINET AND ONE VEHICLE SIGNAL HEAD ON POLE. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB. INSTALL 123W ARCHAION LARGE LED LUMINAIRE FIXTURE (ARCH-L-PA3-120-740-U-T3-AP-10MSP-PR7) WITH CIMCON NODE AT 35FT MOUNTING HEIGHT WITH 12FT LUMINAIRE ARM. INSTALL ONE HYBRID RADAR/VIDEO DETECTION CAMERA ON LUMINAIRE ARM PER MANUFACTURERS' RECOMMENDATIONS.
- 5 INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY AND ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHUTTON ASSEMBLY PER MANUFACTURERS' RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 6 INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY, ONE COUNTDOWN PEDESTRIAN SIGNAL HEAD, AND ONE VEHICLE SIGNAL HEAD ON POLE. INSTALL INTERFACE UNIT IN COUNTDOWN PEDESTRIAN SIGNAL HEAD AND ROUTE CONDUCTOR TO APS PEDESTRIAN PUSHUTTON ASSEMBLY PER MANUFACTURERS' RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 7 INSTALL TYPE PPB POST WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11 AND J-20.26. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY ON POST. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 8 INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL TWO COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ONE VEHICLE SIGNAL HEAD ON POLE. INSTALL INTERFACE UNITS IN COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ROUTE CONDUCTORS TO APS PEDESTRIAN PUSHUTTON ASSEMBLIES PER MANUFACTURERS' RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 9 INSTALL TYPE PPB POST WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11 AND J-20.26. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY ON POST. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 10 INSTALL TYPE I SIGNAL STANDARD WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11, J-20.26, J-21.15, AND J-21.20. INSTALL TWO APS PEDESTRIAN PUSHBUTTON ASSEMBLIES WITH ADAPTERS, AS NEEDED, AND AUDIBLE SPEECH MESSAGES, TWO COUNTDOWN PEDESTRIAN SIGNAL HEADS, AND ONE VEHICLE SIGNAL HEAD ON POLE. INSTALL INTERFACE UNITS IN COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ROUTE CONDUCTORS TO APS PEDESTRIAN PUSHUTTON ASSEMBLIES PER MANUFACTURERS' RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 11 INSTALL TYPE PPB POST WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11 AND J-20.26. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY ON POST.
- 12 INSTALL TYPE I SIGNAL STANDARD AND FOUNDATION PER WSDOT STANDARD PLANS J-20.26, J-21.10, J-21.15, AND J-21.20. INSTALL TWO COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ONE VEHICLE SIGNAL HEAD ON POLE. INSTALL INTERFACE UNITS IN COUNTDOWN PEDESTRIAN SIGNAL HEADS AND ROUTE CONDUCTORS TO APS PEDESTRIAN PUSHUTTON ASSEMBLIES PER MANUFACTURERS' RECOMMENDATIONS. TOP OF FOUNDATION SHALL BE FLUSH WITH ADJACENT FINISHED SIDEWALK OR CURB.
- 13 INSTALL TYPE PPB POST WITH CURB BASE AND FOUNDATION PER WSDOT STANDARD PLANS J-20.11 AND J-20.26. INSTALL ONE APS PEDESTRIAN PUSHBUTTON ASSEMBLY ON POST.

CONSTRUCTION NOTES (SHEET 63 ONLY)

- 16 REMOVE EXISTING POLE AND ALL ASSOCIATED EQUIPMENT AND SALVAGE TO KING COUNTY SIGNAL SHOP. REMOVE EXISTING FOUNDATION, CONDUIT UPSWEEPS, AND ASSOCIATED WIRING. BACKFILL AND COMPACT VOIDS.
- 17 REMOVE EXISTING CABINET AND ALL ASSOCIATED EQUIPMENT AND SALVAGE TO KING COUNTY SIGNAL SHOP. REMOVE EXISTING FOUNDATION, CONDUIT UPSWEEPS, AND ASSOCIATED WIRING. BACKFILL AND COMPACT VOIDS.
- 18 INSTALL TYPE P TRAFFIC SIGNAL CONTROLLER CABINET, BATTERY BACKUP SYSTEM CABINET, ELECTRICAL SERVICE CABINET, AND FOUNDATION PER CITY OF FEDERAL WAY STANDARD DRAWING 3-45B AND DETAILS ON SHEET 68. COORDINATE ELECTRICAL SERVICE CONNECTION WITH PUGET SOUND ENERGY.
- 19 ABANDON EXISTING INDUCTION LOOP DETECTOR. REMOVE EXISTING LOOP LEAD-IN CABLES BACK TO EXISTING TRAFFIC SIGNAL CONTROLLER CABINET.
- 20 EXISTING LUMINAIRE ON EXISTING UTILITY POLE TO REMAIN.
- 21 COORDINATE WITH THE CITY OF FEDERAL WAY AND PUGET SOUND ENERGY REGARDING THE EXISTING OVERHEAD POWER AND COMMUNICATIONS LINES WITHIN THE VICINITY OF THE INTERSECTION. THE EXISTING UTILITY POLES MAY NEED TO BE REPLACED/RELOCATED, AND/OR THE EXISTING OVERHEAD POWER AND COMMUNICATIONS LINES RAISED, TO MAINTAIN ADEQUATE CLEARANCES TO THE TRAFFIC SIGNAL AND ILLUMINATION EQUIPMENT, AND TO ENSURE THE OVERHEAD LINES DO NOT OBSTRUCT THE TRAFFIC SIGNAL INDICATIONS.
- 22 REMOVE EXISTING JUNCTION BOX, CONDUIT UPSWEEPS, AND ASSOCIATED WIRING. BACKFILL AND COMPACT VOIDS. ABANDON UN-USED CONDUITS.
- 23 REPLACE EXISTING JUNCTION BOX WITH TYPE 1 JUNCTION BOX WITH NON-SLIP LID AND FRAME PER WSDOT STANDARD PLAN J-40.10. ADJUST TO MATCH FINISHED GRADE.
- 24 MAINTAIN AND PROTECT EXISTING SMALL CABLE VAULT AND ALL ASSOCIATED CONDUITS AND WIRING. ADJUST EXISTING SMALL CABLE VAULT TO FINISHED GRADE.

SIGN LEGEND (SHEET 63 ONLY)


FONT SERIES D
12" UPPER CASE
9" LOWER CASE




1 2
D3-101 (MOD)
W/G
84" X 18"



3 4
D3-101 (MOD)
W/G/BR
96" X 24"



5 6 7
R3-5R
B/W
30" X 36"



8
R10-11a
B/W
30" X 36"

* SIGN FABRICATION AS PER CITY OF FEDERAL WAY STANDARD DETAILS 3-49A AND 3-54.



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REVIEWED: JC
APPROVED: RWP



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S 288TH ST ROAD DIET - PHASE 2

**TRAFFIC SIGNAL NOTES
MILITARY RD S & S 288TH ST**

CITY PROJECT #:
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WIRING SCHEDULE (NEXT SHEET ONLY)

NO	RACEWAY CONDUIT SIZE	PPB 2C(SH)		EVP IND. 2C		EVP DETECT 3C(SH)		VIDEO DETECT		PED HEAD COMM 4C		PED HEAD 5C		VEH HEAD 5C		VEH HEAD 7C		CCTV CAT6		WIRELESS ANTENNA CAT6		ILLUM #8		SIGNAL POWER #6		PSE SERVICE		CONDUIT FILL %	
		EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP	EX	PROP		
1	2" SCH40																											5.7%	
2	3" SCH40				1		1		1						2		1											12.3%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
3	2" SCH40		1											1														14.0%	
4	2" SCH40		1											1														8.9%	
5	2" SCH80																							2				6.5%	
6	2" SCH80		2				1		1										2		1							18.5%	
6	3" SCH80				1									2		2		2										16.3%	
	2" SCH80	SPARE (EMPTY WITH PULL TAPE)																											
7	3" SCH40				1		1		1						1		1											8.1%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
8	2" SCH40																									3		9.3%	
9	2" SCH40											2		2				1										19.2%	
10	2" SCH40		1									1																6.4%	
11	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
12	2" SCH80		2				1		1																			12.8%	
12	3" SCH80				1									2		2		2										16.3%	
	2" SCH80	SPARE (EMPTY WITH PULL TAPE)																											
13	3" SCH40				1		1		1						2		1											10.0%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
14	2" SCH40						1		1																			5.7%	
14	2" SCH40				1										2		1											18.3%	
15	3" SCH80		4				2		2										2		1							13.0%	
15	3" SCH80				2								4															12.4%	
15	3" SCH80													3		4												18.0%	
	2" SCH80	SPARE (EMPTY WITH PULL TAPE)																											
16	2" SCH40		2											2			1											21.0%	
17	2" SCH40		4				2		2										2		1							25.5%	
17	2" SCH40				2									4														24.4%	
17	3" SCH40														3		4											16.0%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
18	3" SCH40				1		1		1						1		1											8.1%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
19	2" SCH40		4				2		2																			20.4%	
19	2" SCH40				2									4														24.4%	
19	3" SCH40														3		4											16.0%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
20	2" SCH40																							2				5.7%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
21	2" SCH40		1									1																6.4%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
22	2" SCH40		2				1		1																			11.2%	
22	3" SCH40				1									2		1		2										12.5%	
23	2" SCH40		4				1		1																			16.6%	
23	3" SCH40				1									4		2		3										20.7%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											
24	3" SCH40		4				2		2											2		1						11.5%	
24	3" SCH40				2									4														11.0%	
24	3" SCH40														3		4											16.0%	
	2" SCH40	SPARE (EMPTY WITH PULL TAPE)																											

* ALL CONDUIT SHALL CONTAIN A GROUND WIRE EQUAL IN SIZE TO THE LARGEST CONDUCTOR (NO. 8 MINIMUM), UNLESS OTHERWISE NOTED. IF ONE DOES NOT EXIST, INSTALL A NEW GROUND WIRE.
 ** INSTALLED BY PSE.



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S 288TH ST ROAD DIET - PHASE 2
TRAFFIC SIGNAL WIRING SCHEDULE
MILITARY RD S & S 288TH ST

CITY PROJECT #: 36238
 SHT. 62
 OF 81

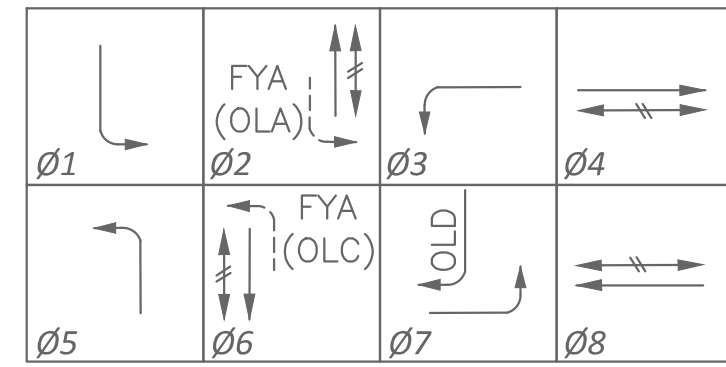
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SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.

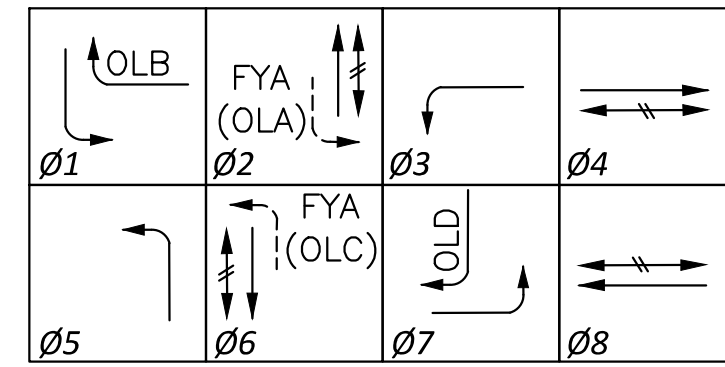
NOTES

- SEE SHEET 53 FOR GENERAL NOTES AND LEGEND.
- SEE SHEET 61 FOR CONSTRUCTION NOTES AND SIGN LEGEND.
- SEE SHEET 62 FOR TRAFFIC SIGNAL WIRING SCHEDULE.
- SEE SHEET 64 FOR TRAFFIC SIGNAL WIRING DIAGRAM.
- SEE SHEETS 65 - 66 FOR TRAFFIC SIGNAL POLE CHART.

SIGNAL PHASING (EX.)



SIGNAL PHASING (PROP.)

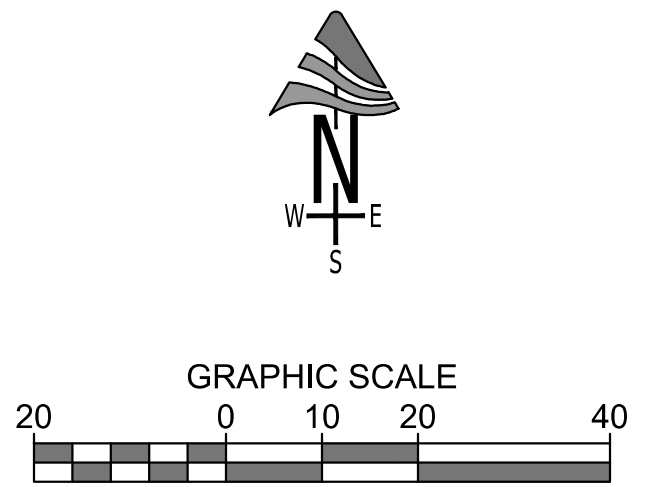
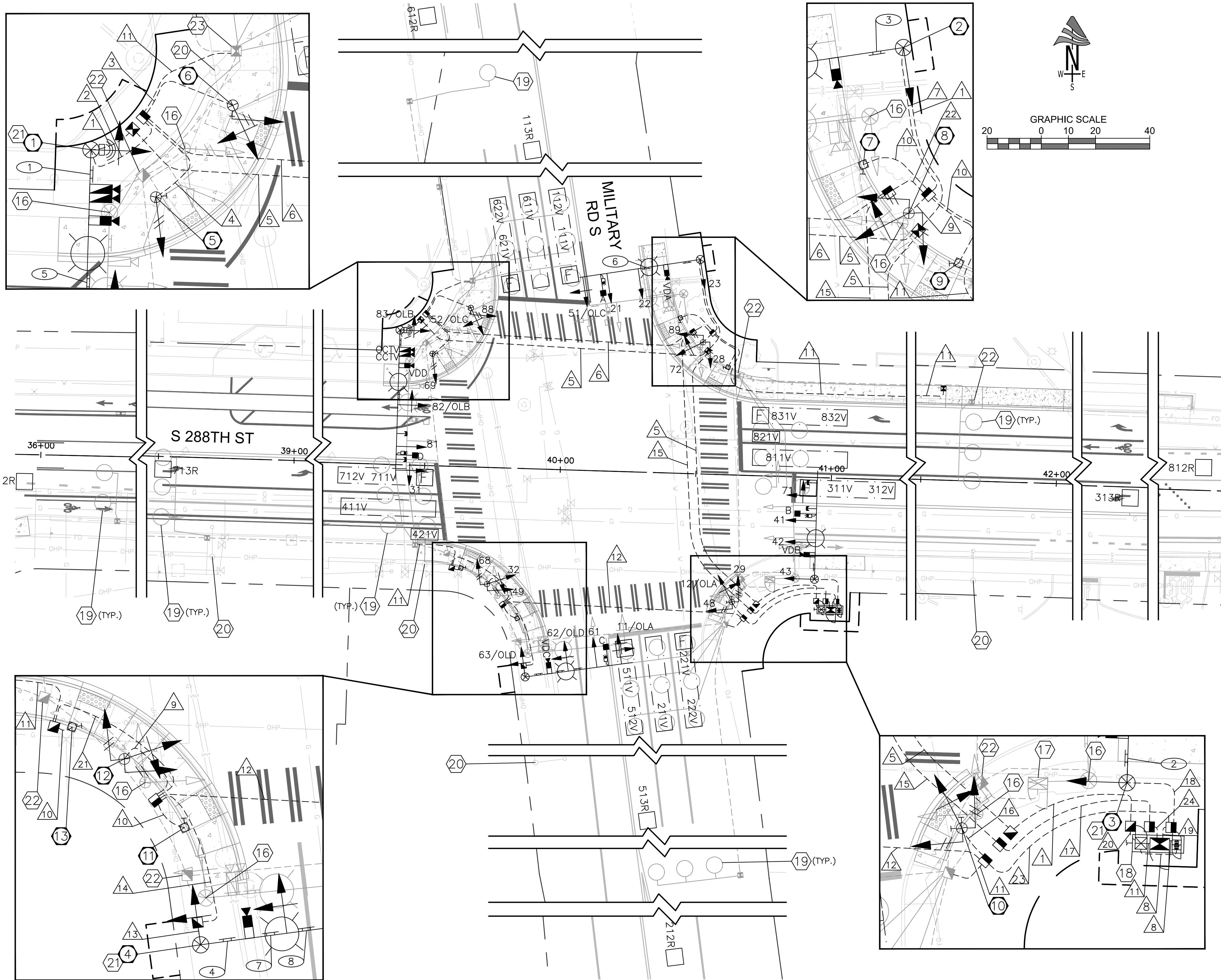
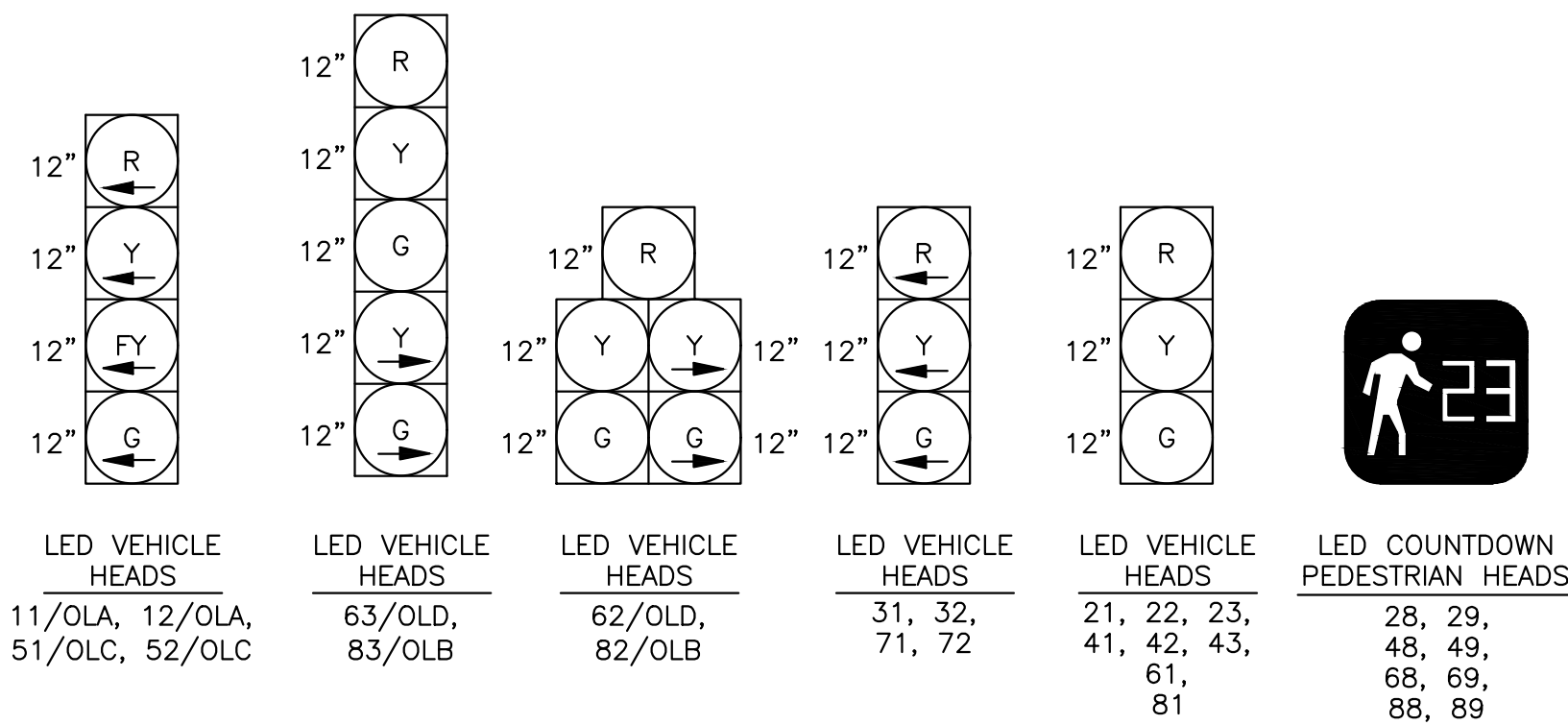


- PROTECTED MOVEMENT
- ⇄ PERMITTED MOVEMENT
- ⇄ PEDESTRIAN MOVEMENT

PRE-EMPTION SCHEDULE (EX. AND PROP.)

EVP	PHASE(S)
A	ø2, ø5
B	ø4, ø7
C	ø1, ø6
D	ø3, ø8

SIGNAL HEAD ASSIGNMENT (THIS SHEET ONLY)



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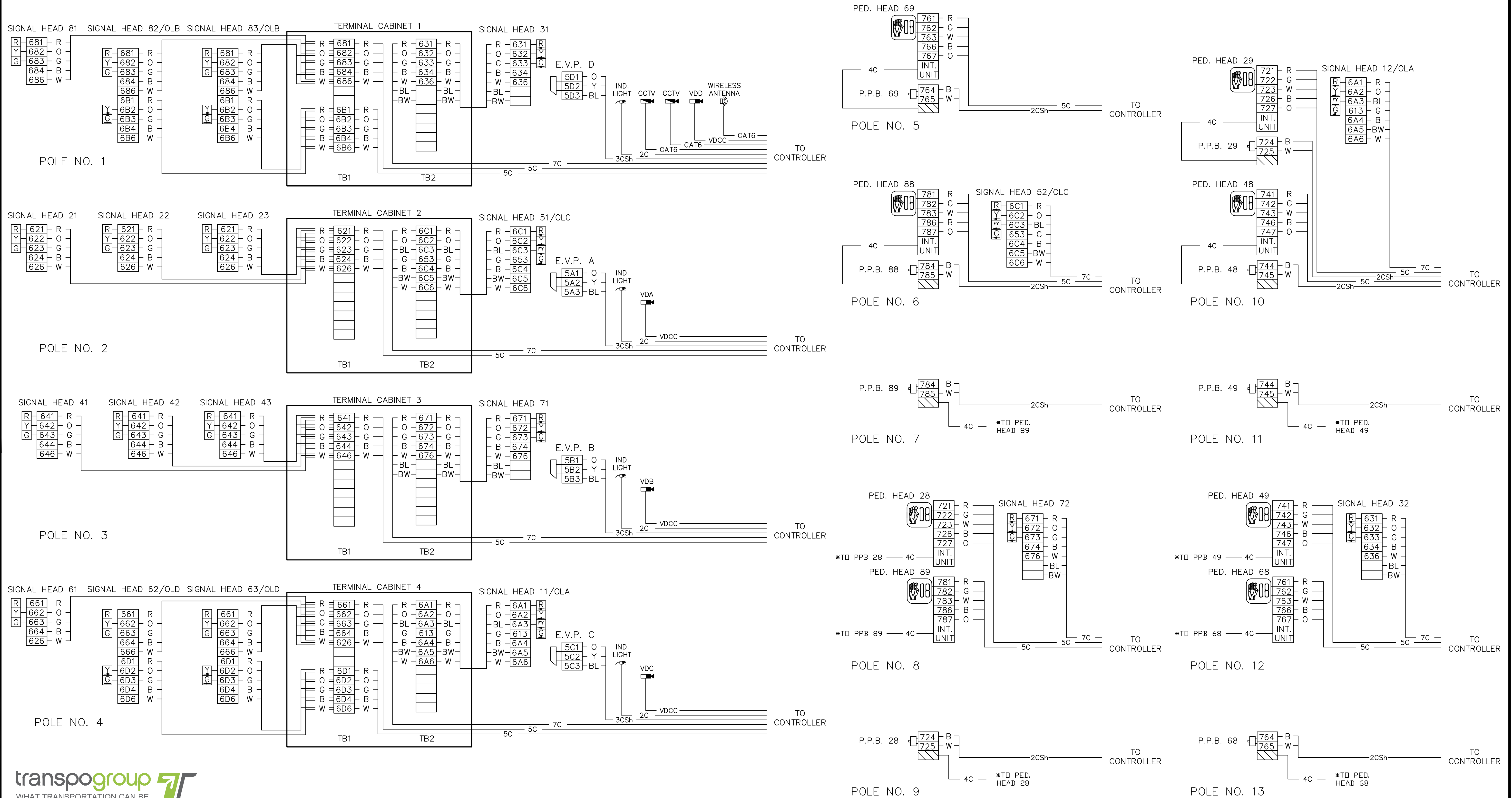
S 288TH ST ROAD DIET - PHASE 2
TRAFFIC SIGNAL PLAN
MILITARY RD S & S 288TH ST

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FIELD WIRE TERMINATIONS



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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC SIGNAL WIRING DIAGRAM MILITARY RD S & S 288TH ST

CITY PROJECT #: 36238
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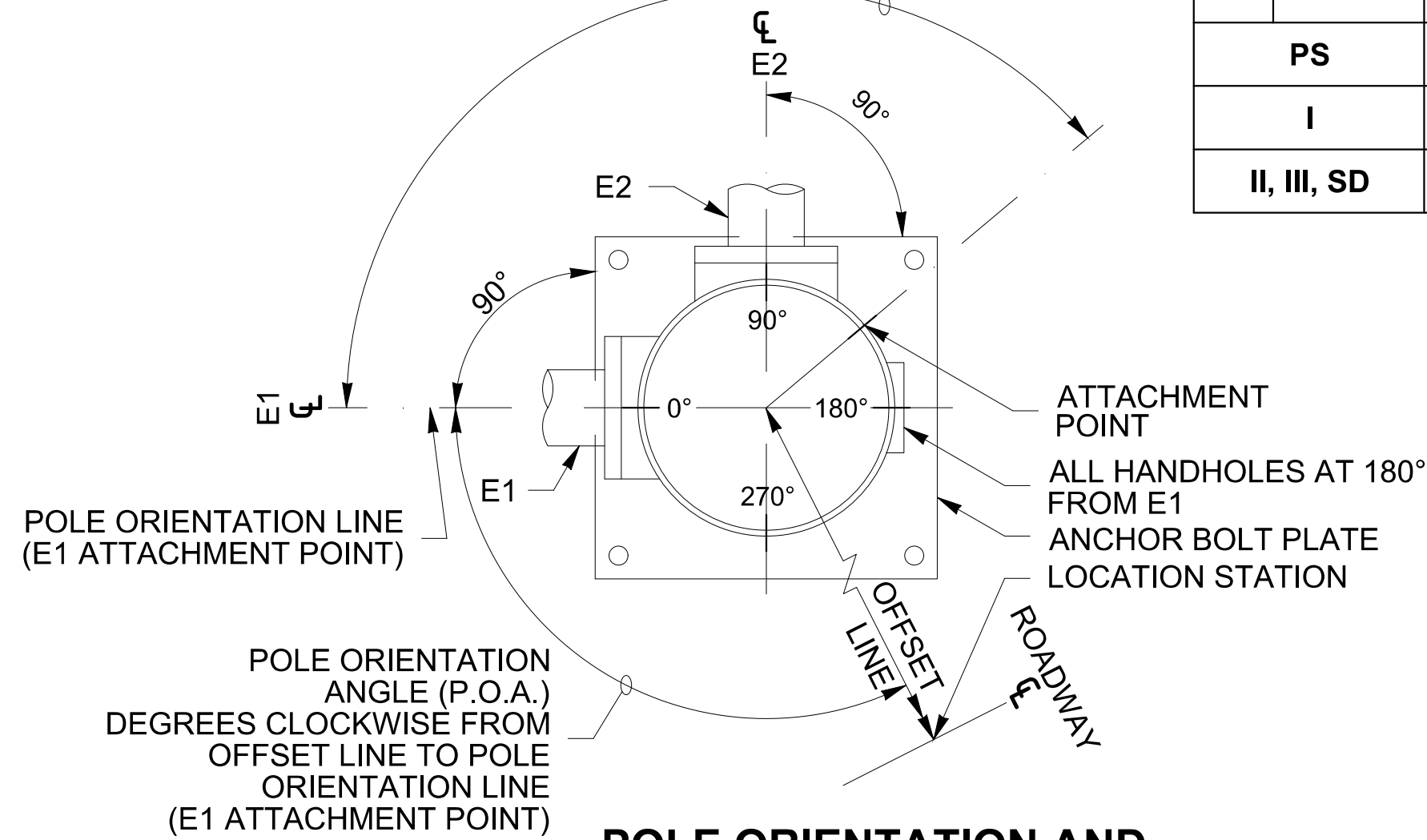
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LEGEND

- a. VEHICLE DISPLAY
- b. MASTARM MTD. SIGN
- c. STREET NAME SIGN
- d. PRE-EMPT DETECTOR
- e. POST MTD. SIGN
- f. LUMINAIRE
- g. PEDESTRIAN DISPLAY
- h. TERMINAL CABINET
- i. APS PPB-M
- j. HANDHOLE
- k. DETECTION CAMERA

MEASURE ATTACHMENT POINT ANGLES CLOCKWISE FROM POLE ORIENTATION LINE (E1 ATTACHMENT POINT) TO THE ATTACHMENT POINT LOCATION



POLE ORIENTATION AND ATTACHMENT POINT DETAIL

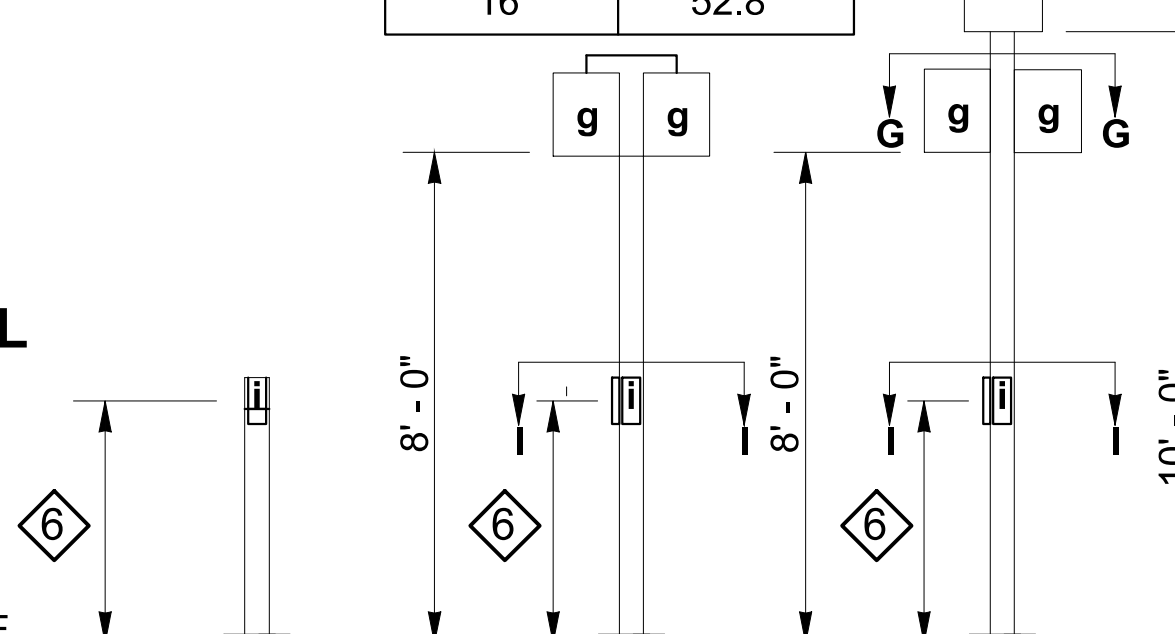
NOTE:
TYPE E MOUNTS SHALL BE USED FOR PEDESTRIAN DISPLAYS ON TYPE II OR III SIGNAL STANDARDS. WITH THE FOLLOWING EXCEPTION: PEDESTRIAN DISPLAYS MOUNTED ON OCTAGONAL (8 SIDED) SIGNAL STANDARDS AT AN ANGLE OTHER THAN A 45 DEGREE INCREMENT SHALL USE A TYPE A MOUNT FOR TWO PEDESTRIAN DISPLAYS, OR A TYPE B MOUNT FOR A SINGLE PEDESTRIAN DISPLAY.

3' - 6" MEASURED FROM SIDEWALK SURFACE TO CENTER OF PEDESTRIAN PUSH BUTTON

TYPE PPB
PPB POST

TYPE PS
PED. HEAD
STANDARD

TYPE I
VEHICLE HEAD
STANDARD



STANDARD TYPE	STANDARD PLAN REFERENCES		
	POLE	FOUNDATION STANDARD	ELECTRICAL CURB
PPB	FIXED	J-20.10	J-20.10
	BREAKAWAY	J-20.15	J-20.15
PS	J-20.16	J-21.10	J-20.20
I	J-21.15	J-21.10	J-21.20
II, III, SD	N/A	J-26.10, J-26.15	N/A

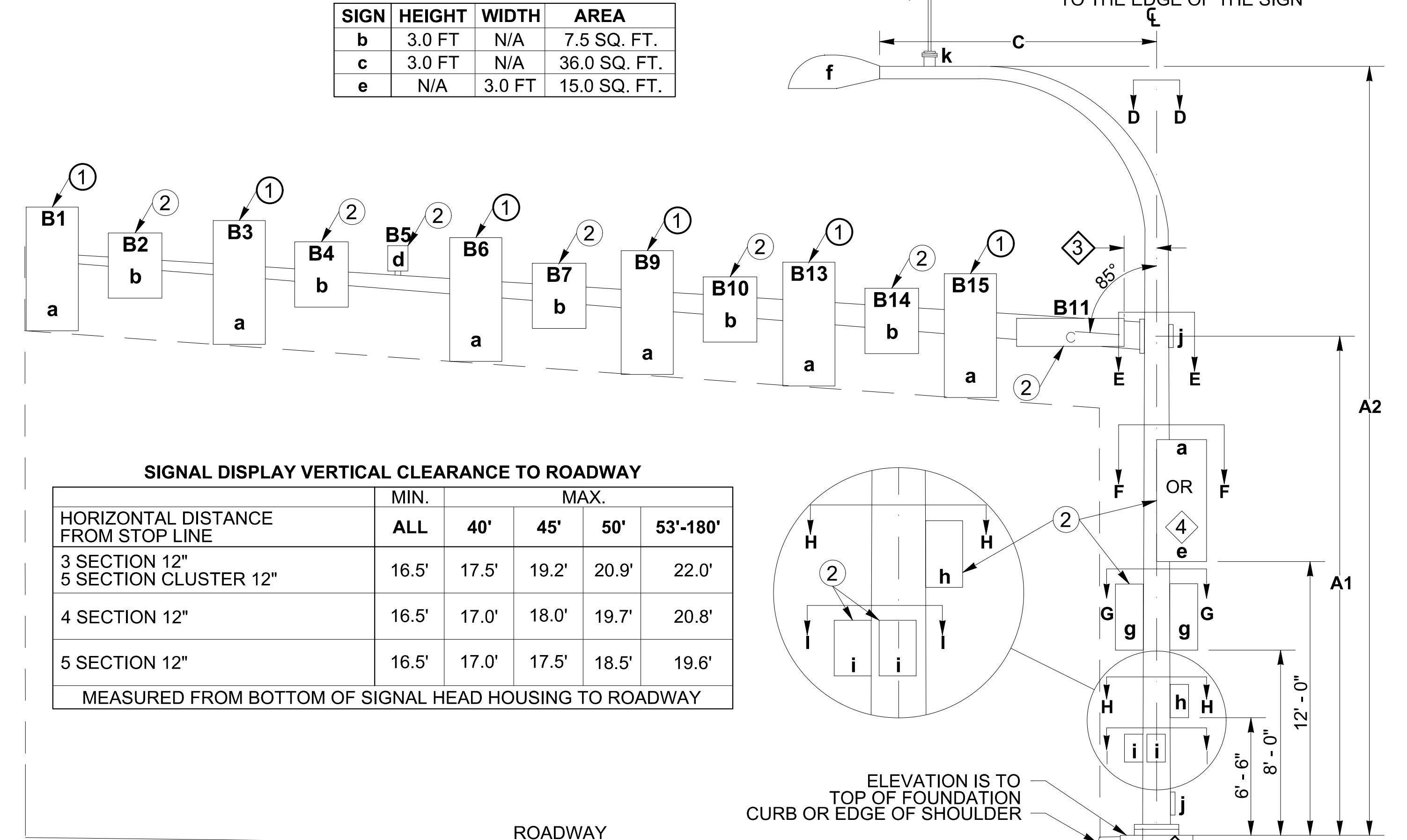
LUMINAIRE MASTARM X Y Z (ft³)	
mastarm LENGTH	X Y Z (ft³)
6'	19.8
8'	26.4
10'	33.0
12'	39.6
14'	46.2
16'	52.8

NOTES

- MOUNTING COUPLING INSTALLED AT OFFSET DISTANCE INDICATED IN CHART. FOR TYPE N MOUNTS ONLY, DRILL 1" DIA. HOLE IN MASTARM AND INSTALL PLASTIC SPLIT BUSHING FOR CABLE ENTRANCE.
- FIELD INSTALLED. SIGN SIZES SHALL NOT EXCEED THE MAXIMUM VALUES LISTED HERE:

SIGN	HEIGHT	WIDTH	AREA
b	3.0 FT	N/A	7.5 SQ. FT.
c	3.0 FT	N/A	36.0 SQ. FT.
e	N/A	3.0 FT	15.0 SQ. FT.

- 1' - 0" MIN. TO 2' - 6" MAX. FROM POLE CENTERLINE TO SIGN EDGE
- FOR POST MOUNTED SIGNS THERE SHALL BE 2' - 0" MIN. FROM THE FACE OF THE CURB OR THE EDGE OF THE SHOULDER TO THE EDGE OF THE SIGN
- MOUNT PER MANUFACTURER'S RECOMMENDATION



HORIZONTAL DISTANCE FROM STOP LINE	SIGNAL DISPLAY VERTICAL CLEARANCE TO ROADWAY				
	MIN.	MAX.			
3 SECTION 12"	ALL	40'	45'	50'	53'-180'
5 SECTION CLUSTER 12"	16.5'	17.5'	19.2'	20.9'	22.0'
4 SECTION 12"	16.5'	17.0'	18.0'	19.7'	20.8'
5 SECTION 12"	16.5'	17.0'	17.5'	18.5'	19.6'

MEASURED FROM BOTTOM OF SIGNAL HEAD HOUSING TO ROADWAY

- DMS OR VMS IS NOT ALLOWED ON THESE POLES.
- TYPE II, III & SD SIGNAL STANDARD
- PLACEMENT SHALL BE 2' - 0" MIN. FROM FACE OF CURB OR EDGE OF SHOULDER; 3' - 0" MIN. FROM FACE OF GUARDRAIL; 4' - 0" MIN. FROM CONC. BARRIER TYPE 2 (MEASURED FROM A POINT WHERE THE BARRIER BASE MEETS THE SHOULDER SURFACE (TOE). MEASUREMENT TAKEN FROM TRAFFIC SIDE OF BARRIER; TO FACE OF POLE)



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(TEL) 425 821-3665
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DESIGNED: TK
REVIEWED: JC
APPROVED: RWP



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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC SIGNAL POLE CHART

CITY PROJECT #: 36238
SHT. 65 OF 81

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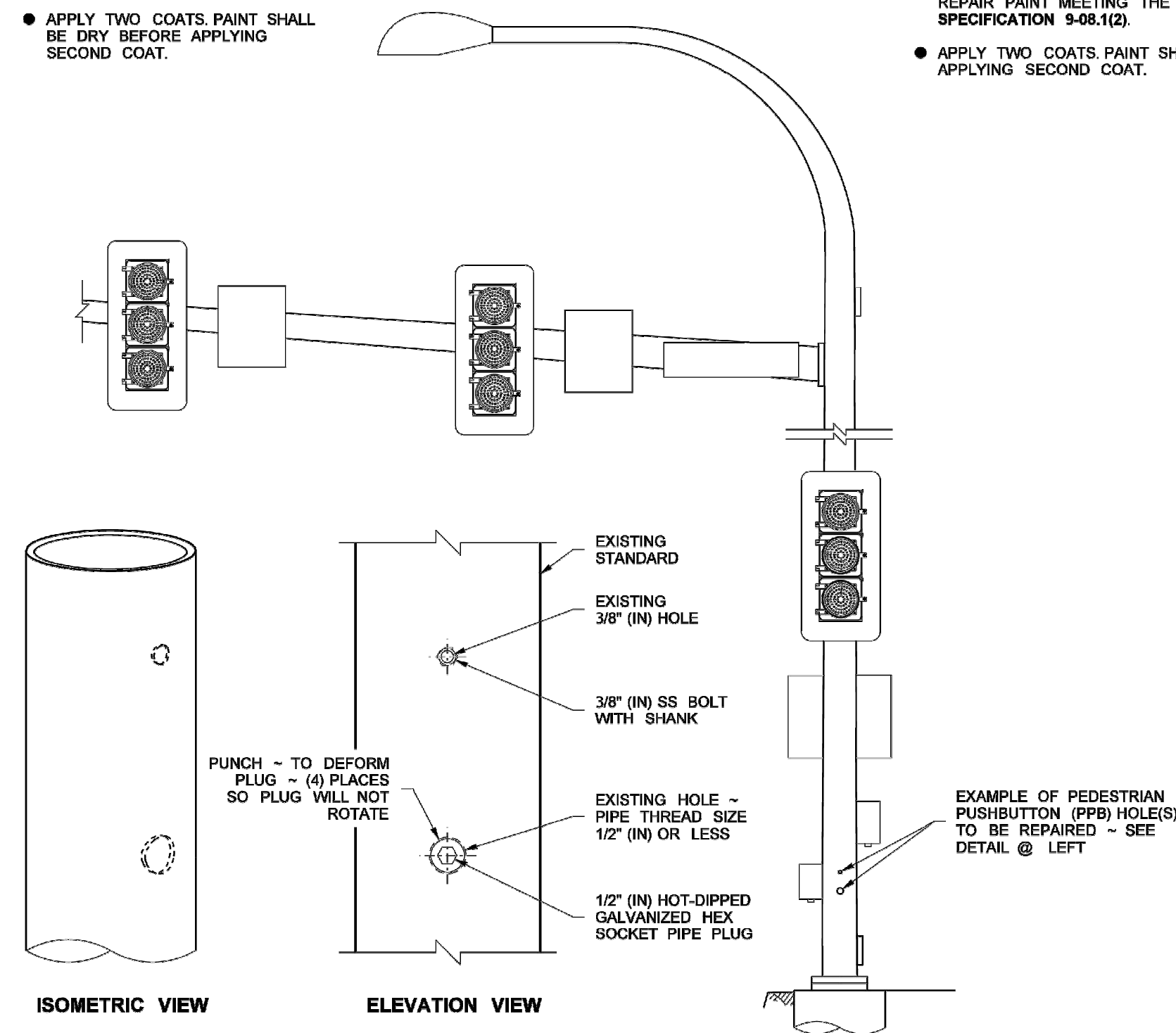
**REPAIR PROCEDURE - BOLT HOLE SIZE 1/2" (IN) DIAMETER OR LESS ~
PIPE TRADE SIZE 1/2" (IN) OR LESS**

FOR UNC OR UNF THREADS:

- APPLY SILICONE CAULK TO THREADS OF SS BOLT WITH SHANK.
- TIGHTEN SS BOLT UNTIL SHANK IS TIGHT AGAINST STANDARD.
- CUT OFF SS BOLT EVEN WITH STANDARD.
- FILE SS BOLT TO MATCH CONTOUR OF STANDARD.
- TREAT SS BOLT AND SURROUNDING POLE WITH GALVANIZING REPAIR PAINT MEETING THE REQUIREMENT OF STANDARD SPECIFICATION 9-08.1(2).
- APPLY TWO COATS. PAINT SHALL BE DRY BEFORE APPLYING SECOND COAT.

FOR NATIONAL PIPE THREADS:

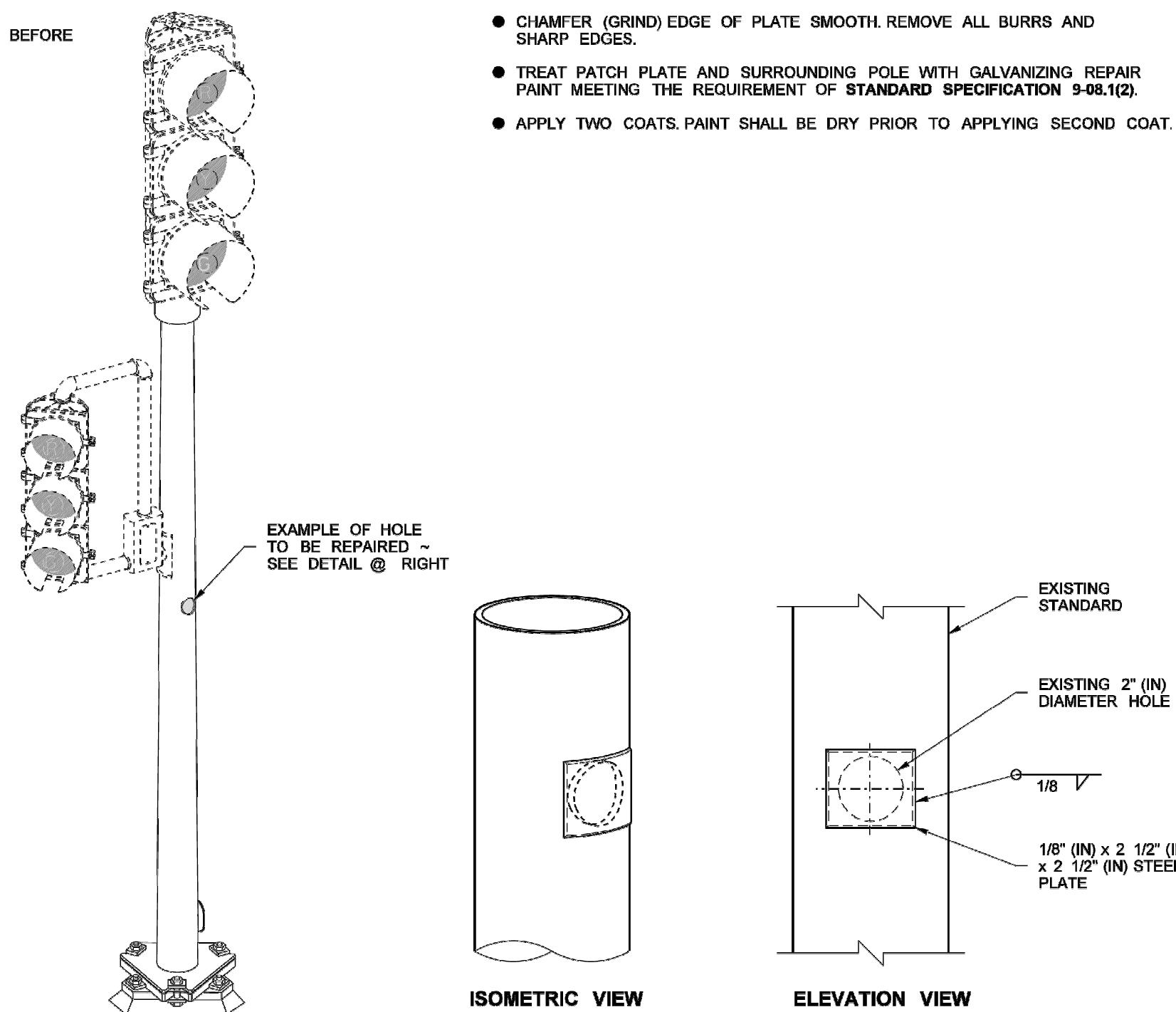
- APPLY SILICONE CAULK TO THREADS OF HOT-DIPPED GALVANIZED HEX SOCKET PIPE PLUG.
- TIGHTEN PLUG UNTIL FLUSH WITH STANDARD.
- FILE PLUG TO MATCH CONTOUR OF STANDARD.
- FILL HEX SOCKET WITH PAINTABLE SILICONE CAULK.
- TREAT PLUG AND SURROUNDING POLE WITH GALVANIZING REPAIR PAINT MEETING THE REQUIREMENT OF STANDARD SPECIFICATION 9-08.1(2).
- APPLY TWO COATS. PAINT SHALL BE DRY BEFORE APPLYING SECOND COAT.



HOLE REPAIR DETAIL
EXAMPLE OF REPAIR FROM PPB LOCATION ~
1/2" (IN) DIAMETER AND 3/8" (IN) DIAMETER HOLES SHOWN (TYPE III SIGNAL STANDARD SHOWN)

REPAIR PROCEDURE - MAXIMUM HOLE SIZE 3" (IN) DIAMETER OR LESS

- SUBMITTAL PER STANDARD SPECIFICATION 6-03.3(25).
- MAXIMUM HOLE SIZE 3" (IN) OR LESS IN DIAMETER.
- 1/8" (IN) STEEL PLATE - TYPE ASTM A - 36 ELECTRODE TYPE XX-70.
- THE PLATE SHALL BE 1/2" (IN) LARGER THAN THE DIAMETER OF THE HOLE TO BE REPAIRED (1/4" (IN) OVERLAP ALL SIDES).
- TACK WELD PLATE TO POLE.
- SHAPE PLATE TO MATCH TAPER AND CURVE OF POLE.
- 1/8" (IN) FILLET WELD ALL AROUND.
- CHAMFER (GRIND) EDGE OF PLATE SMOOTH. REMOVE ALL BURRS AND SHARP EDGES.
- TREAT PATCH PLATE AND SURROUNDING POLE WITH GALVANIZING REPAIR PAINT MEETING THE REQUIREMENT OF STANDARD SPECIFICATION 9-08.1(2).
- APPLY TWO COATS. PAINT SHALL BE DRY PRIOR TO APPLYING SECOND COAT.



PERSPECTIVE VIEW
(RAMP METER SIGNAL STANDARD WITH SLIP BASE SHOWN)
HOLE REPAIR DETAIL
EXAMPLE WITH 2" (IN) DIAMETER HOLE SHOWN

FILE NAME	S:\Design R P& S\4-Standards\2-Plan Sheet Library\01-Published PSL(VIS) Illumination, Signals, and ITS(VIS-17) Traffic Standard Hand Hole Repair Procedure\IS-17.dgn	REGION NO.	STATE	FED.AID PROJ.NO.		Plot 1
TIME	1:41:37 PM	WASH				PLAN REF NO
DATE	10/6/2014					IS-17
PLOTTED BY	FletCo	JOB NUMBER				SHEET
DESIGNED BY		CONTRACT NO.				1
ENTERED BY		LOCATION NO.				OF
CHECKED BY						1
PROJ. ENGR.						SHEETS
REGIONAL ADM.		REVISION	DATE	BY		



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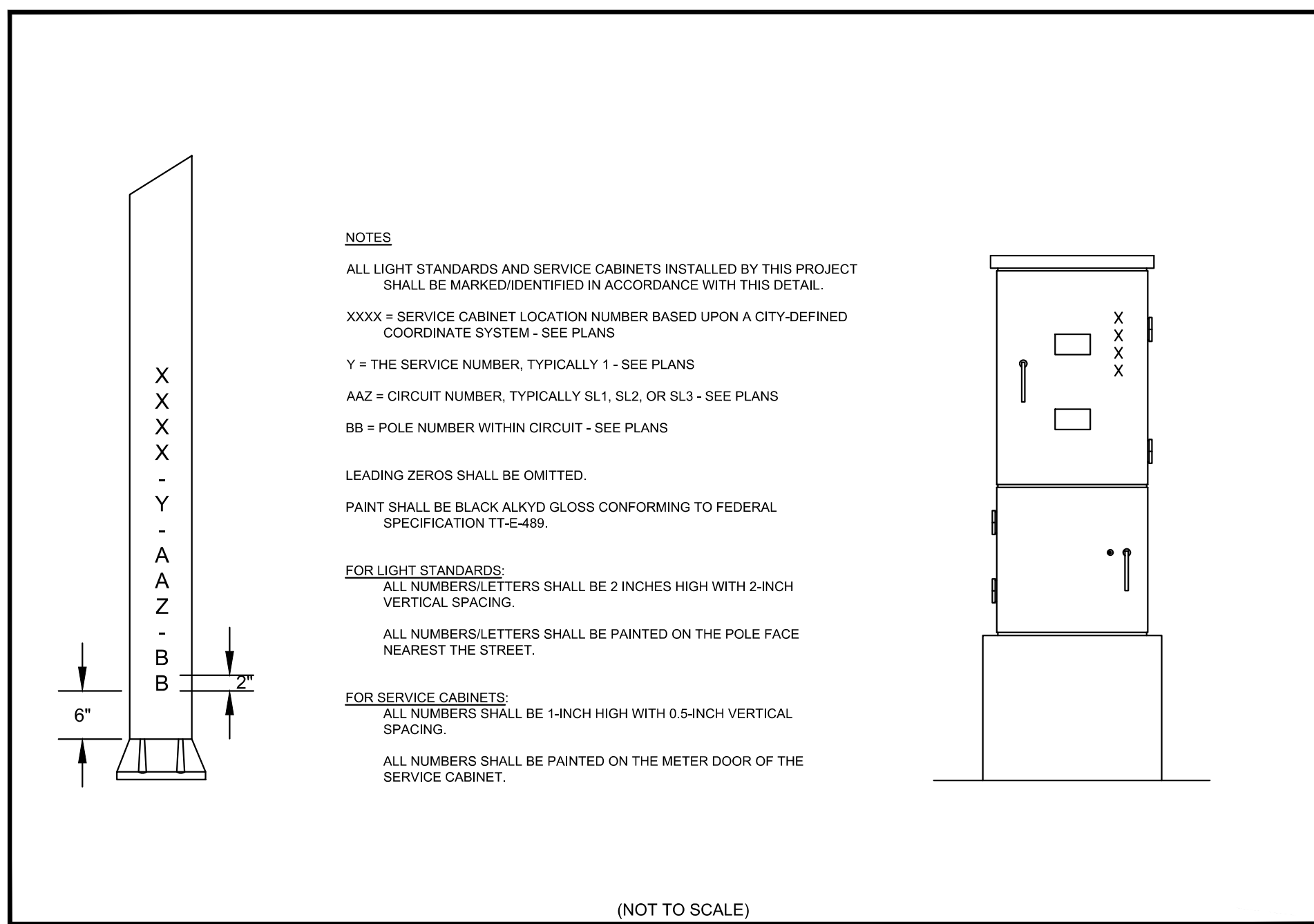
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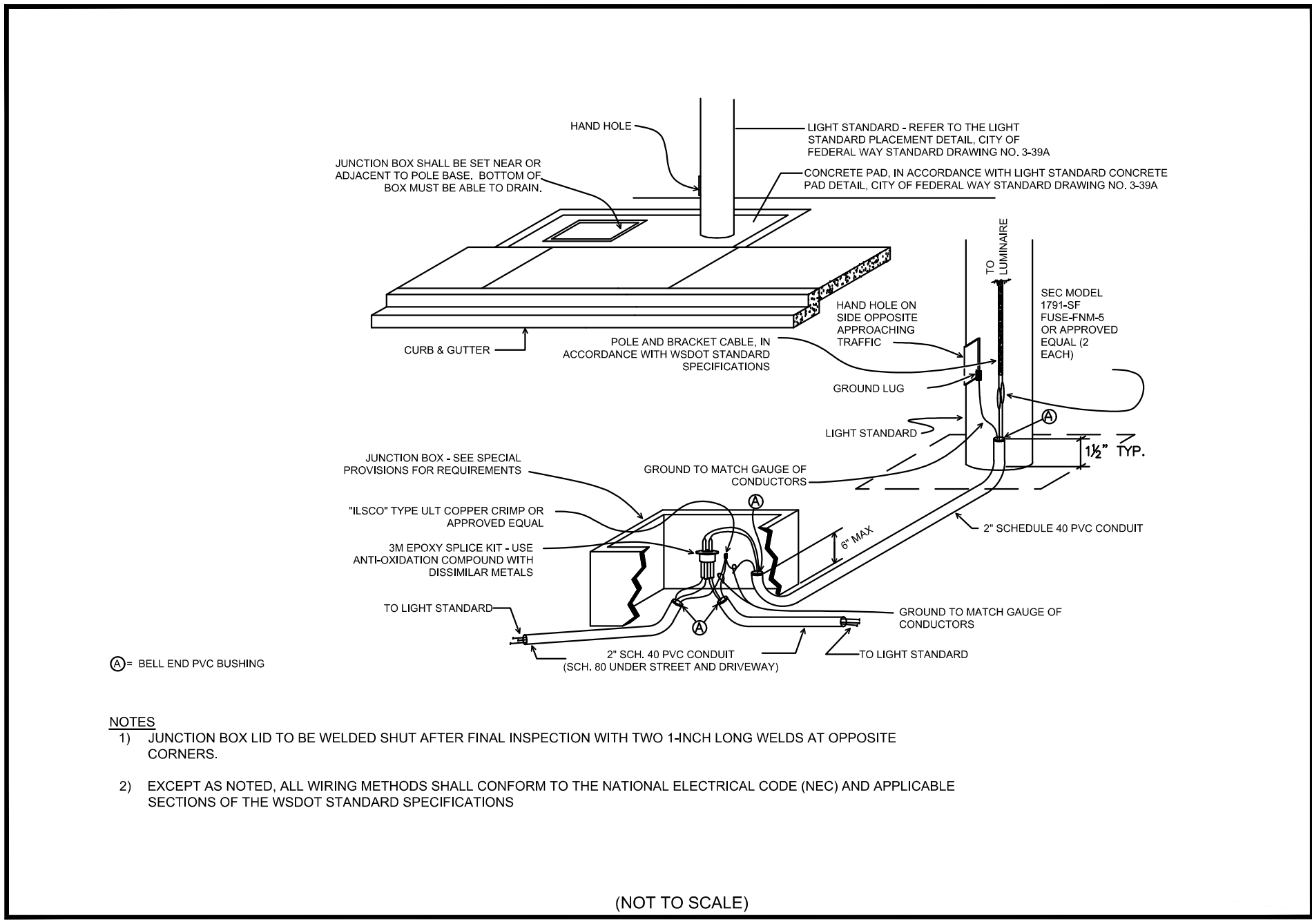
TRAFFIC SIGNAL DETAILS

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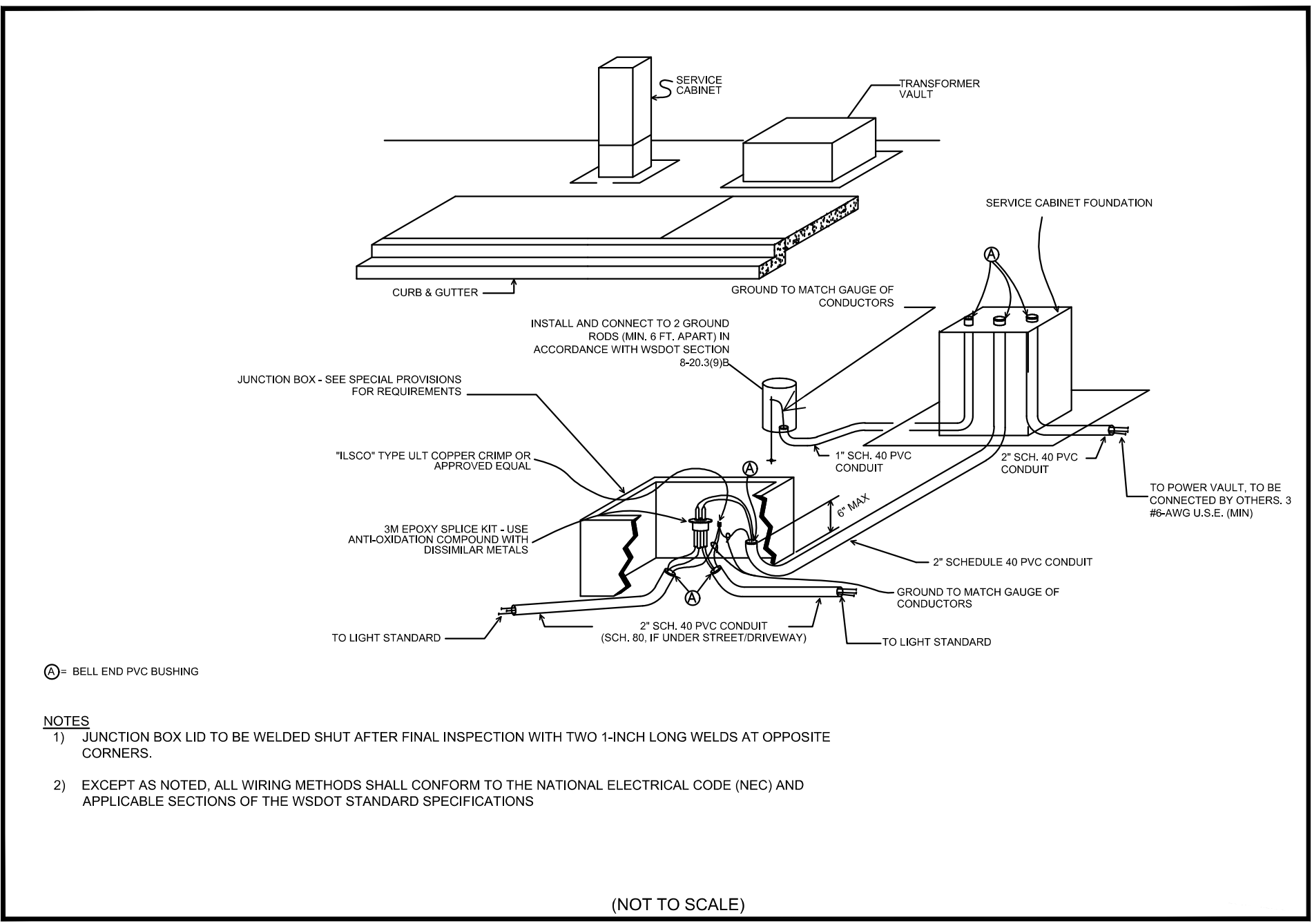
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CITY OF Federal Way PUBLIC WORKS LIGHT STANDARD AND SERVICE CABINET NUMBERING



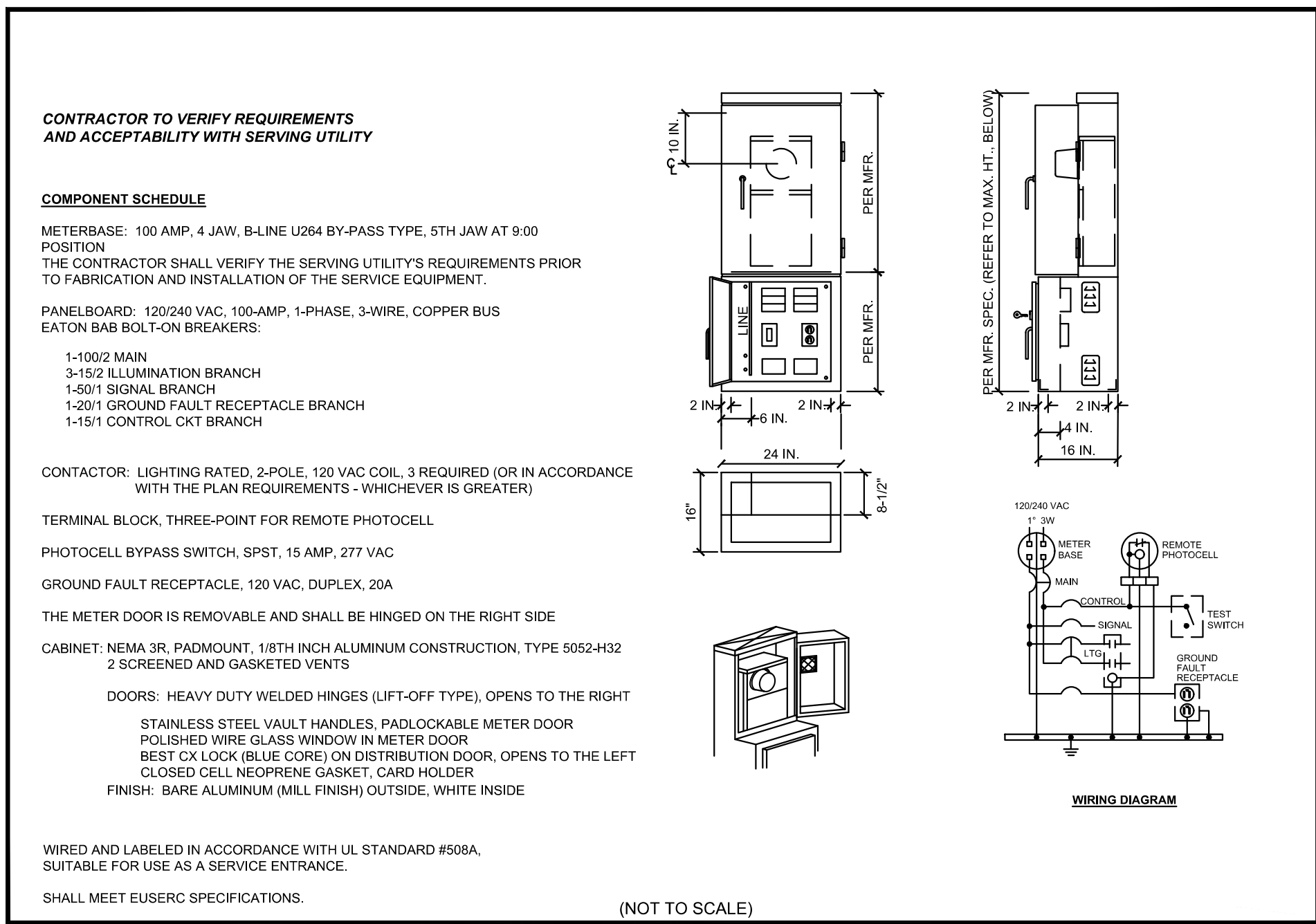
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CITY OF Federal Way PUBLIC WORKS UNIFORM LUMINAIRE WIRING DETAIL



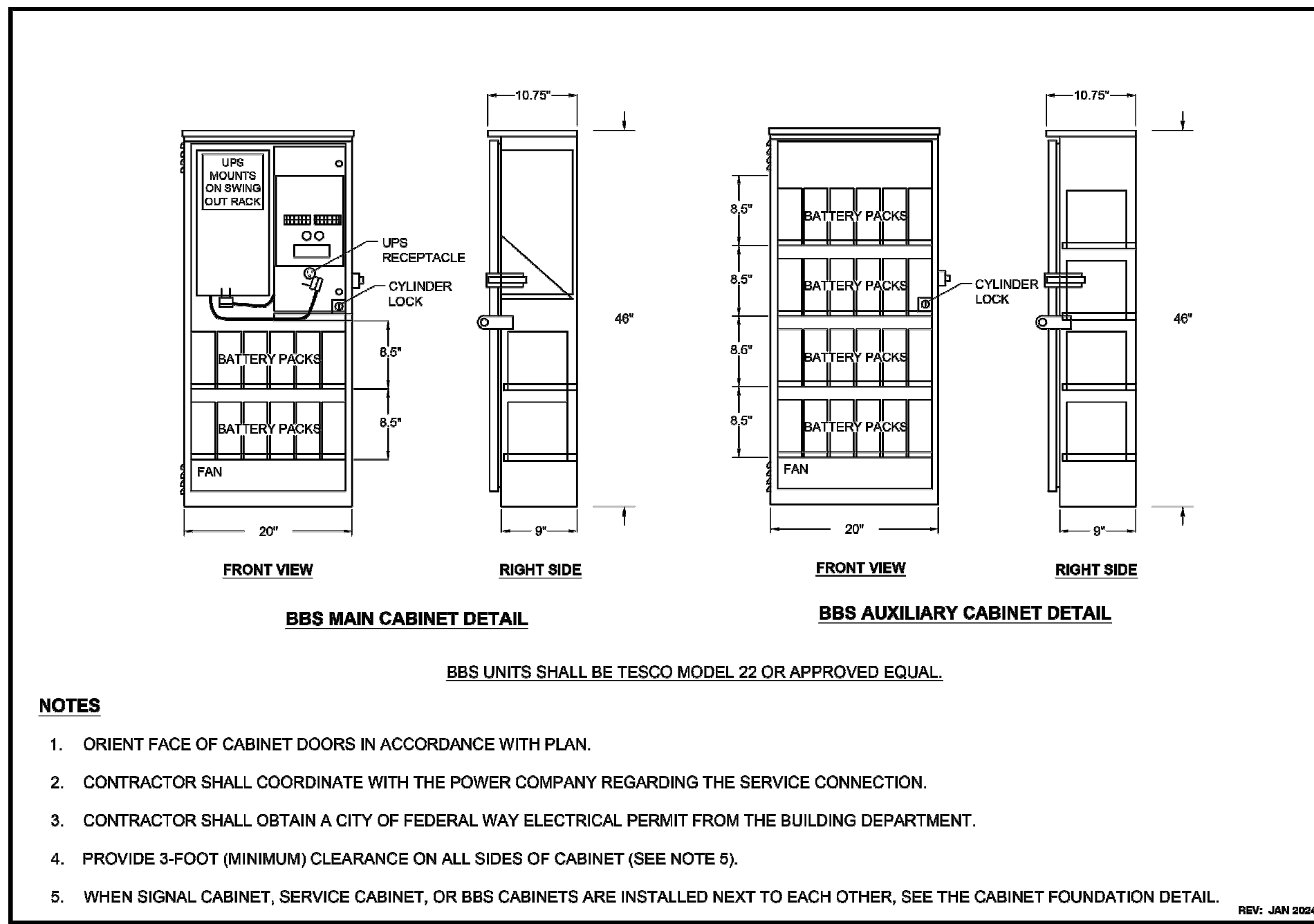
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CITY OF Federal Way PUBLIC WORKS ILLUMINATION SYSTEM SERVICE DETAIL



(NOT TO SCALE)

CITY OF Federal Way PUBLIC WORKS STREET LIGHT SERVICE CABINET

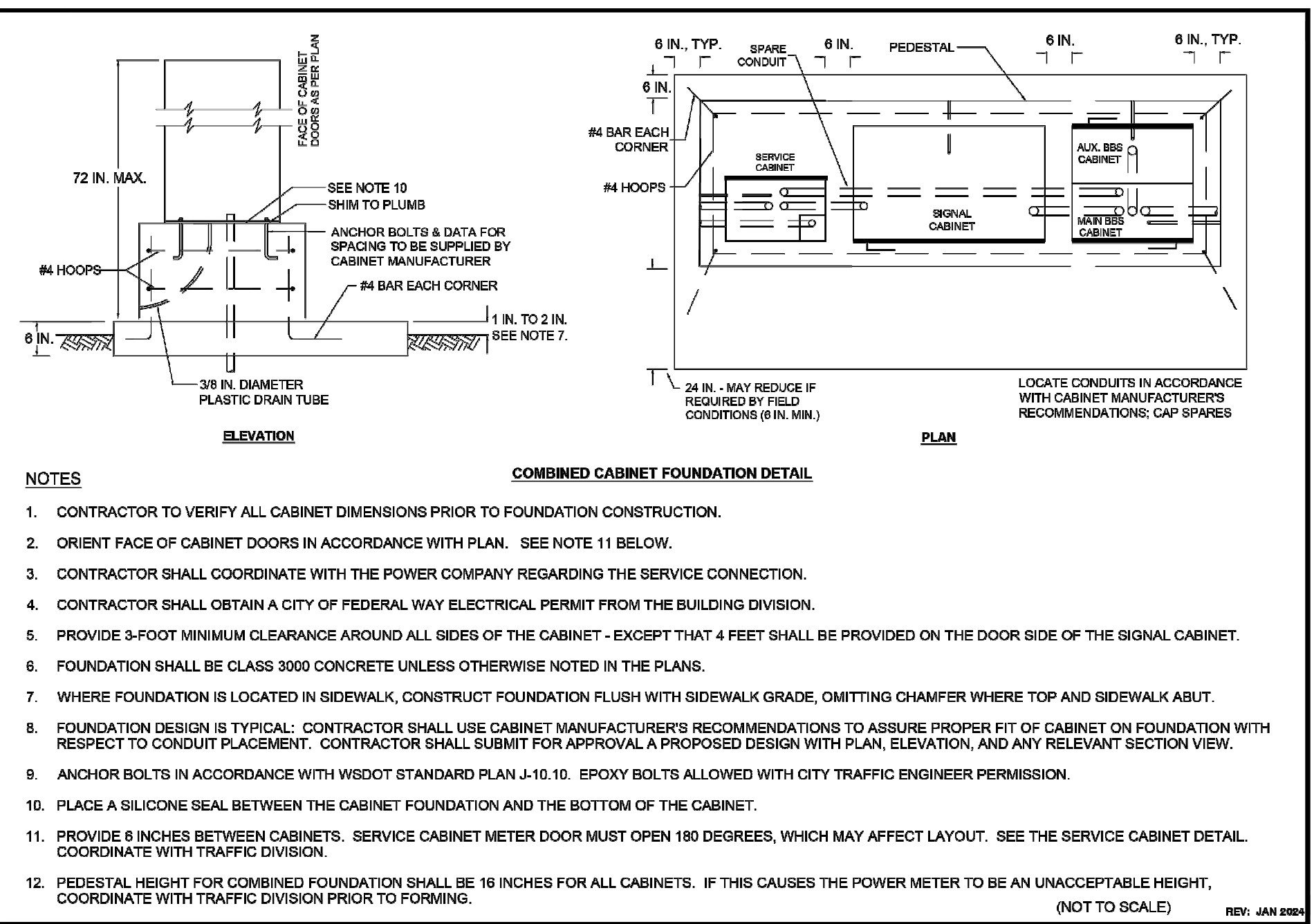


BBS UNITS SHALL BE TESCO MODEL 22 OR APPROVED EQUAL.

- NOTES**
- ORIENT FACE OF CABINET DOORS IN ACCORDANCE WITH PLAN.
 - CONTRACTOR SHALL COORDINATE WITH THE POWER COMPANY REGARDING THE SERVICE CONNECTION.
 - CONTRACTOR SHALL OBTAIN A CITY OF FEDERAL WAY ELECTRICAL PERMIT FROM THE BUILDING DEPARTMENT.
 - PROVIDE 3-FOOT (MINIMUM) CLEARANCE ON ALL SIDES OF CABINET (SEE NOTE 5).
 - WHEN SIGNAL CABINET, SERVICE CABINET, OR BBS CABINETS ARE INSTALLED NEXT TO EACH OTHER, SEE THE CABINET FOUNDATION DETAIL.

REV: JAN 2024

CITY OF Federal Way PUBLIC WORKS BATTERY BACKUP SYSTEM (BBS) CABINETS



- NOTES**
- CONTRACTOR TO VERIFY ALL CABINET DIMENSIONS PRIOR TO FOUNDATION CONSTRUCTION.
 - ORIENT FACE OF CABINET DOORS IN ACCORDANCE WITH PLAN. SEE NOTE 11 BELOW.
 - CONTRACTOR SHALL COORDINATE WITH THE POWER COMPANY REGARDING THE SERVICE CONNECTION.
 - CONTRACTOR SHALL OBTAIN A CITY OF FEDERAL WAY ELECTRICAL PERMIT FROM THE BUILDING DIVISION.
 - PROVIDE 3-FOOT MINIMUM CLEARANCE AROUND ALL SIDES OF THE CABINET - EXCEPT THAT 4 FEET SHALL BE PROVIDED ON THE DOOR SIDE OF THE SIGNAL CABINET.
 - FOUNDATION SHALL BE CLASS 3000 CONCRETE UNLESS OTHERWISE NOTED IN THE PLANS.
 - WHERE FOUNDATION IS LOCATED IN SIDEWALK, CONSTRUCT FOUNDATION FLUSH WITH SIDEWALK GRADE, OMITTING CHAMFER WHERE TOP AND SIDEWALK ABUT.
 - FOUNDATION DESIGN IS TYPICAL; CONTRACTOR SHALL USE CABINET MANUFACTURER'S RECOMMENDATIONS TO ASSURE PROPER FIT OF CABINET ON FOUNDATION WITH RESPECT TO CONDUIT PLACEMENT. CONTRACTOR SHALL SUBMIT FOR APPROVAL A PROPOSED DESIGN WITH PLAN, ELEVATION, AND ANY RELEVANT SECTION VIEW.
 - ANCHOR BOLTS IN ACCORDANCE WITH WSDOT STANDARD PLAN J-10.10. EPOXY BOLTS ALLOWED WITH CITY TRAFFIC ENGINEER PERMISSION.
 - PLACE A SILICONE SEAL BETWEEN THE CABINET FOUNDATION AND THE BOTTOM OF THE CABINET.
 - PROVIDE 8 INCHES BETWEEN CABINETS. SERVICE CABINET METER DOOR MUST OPEN 180 DEGREES, WHICH MAY AFFECT LAYOUT. SEE THE SERVICE CABINET DETAIL. COORDINATE WITH TRAFFIC DIVISION.
 - PEDESTAL HEIGHT FOR COMBINED FOUNDATION SHALL BE 16 INCHES FOR ALL CABINETS. IF THIS CAUSES THE POWER METER TO BE AN UNACCEPTABLE HEIGHT, COORDINATE WITH TRAFFIC DIVISION PRIOR TO FORMING.

REV: JAN 2024

CITY OF Federal Way PUBLIC WORKS COMBINED SERVICE, SIGNAL, AND BBS FOUNDATION



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KIRKLAND, WASHINGTON 98034

(TEL) 425 821-3665
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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC SIGNAL DETAILS

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SHT. 68
OF 81

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GENERAL NOTES

- ALL WORK SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PART 6, CITY OF FEDERAL WAY STANDARD SPECIFICATIONS, AND THE CONTRACT, UNLESS OTHERWISE NOTED.
- A COPY OF THE APPROVED PLAN(S) SHALL BE ON SITE DURING CONSTRUCTION.
- ALL PERMITS REQUIRED FOR WORK WITHIN THE PUBLIC RIGHT-OF-WAY MUST BE OBTAINED PRIOR TO THE START OF CONSTRUCTION.
- POSTED SPEEDS WITHIN THE PROJECT SITE ARE AS FOLLOWS:
 - 288TH ST WEST OF MILITARY ROAD S = 35 MPH
 - 288TH ST EAST OF MILITARY ROAD S = 30 MPH
 - SR 99 (PACIFIC HWY S) = 45 MPH
 - MILITARY RD = 40 MPH
 INSTALL PROPOSED SPEED LIMIT SIGNING ON 288TH STREET WITHIN PROJECT LIMITS PRIOR TO BEGINING CONSTRUCTION.
- NOTIFY THE FOLLOWING AGENCIES A MINIMUM OF 10 CALENDAR DAYS IN ADVANCE OF ANY ROAD CLOSURES OR DETOURS:
 - CITY OF FEDERAL WAY PUBLIC WORKS - (253)835-2700
 - SOUTH KING FIRE AND RESCUE - (253)839-6234
 - CITY OF FEDERAL WAY POLICE DEPARTMENT - (253)835-6700
 - FEDERAL WAY TRANSIT CENTER - (206)553-3000
 - KING COUNTY METRO - CONSTRUCTION.COORD@KINGCOUNTY.GOV
- MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES, UNLESS OTHERWISE NOTED IN THESE PLANS OR APPROVED BY THE ENGINEER.
- THE LOCATION OF EXISTING FEATURES AND UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO COMMENCING CLASS A SIGNING WORK. NOT ALL EXISTING FEATURES AND UTILITIES MAY BE SHOWN.
- ALL WORK SHALL BE CONSISTENT WITH UTILITY AGENCY REQUIREMENTS. THE CONTRACTOR SHALL CONTACT UTILITY AGENCIES 48 HOURS PRIOR TO COMMENCING WORK AND SHALL COORDINATE WITH ALL AFFECTED UTILITY AGENCIES THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE TO EXISTING UTILITIES RESULTING FROM CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL NOTIFY THE AFFECTED UTILITY AGENCY IMMEDIATELY UPON DAMAGE AND SHALL BE RESPONSIBLE FOR REPLACING DAMAGED EQUIPMENT TO THE SATISFACTION OF THE AFFECTED UTILITY AGENCY.
- MAINTAIN ACCESS TO TRANSIT STOPS AT ALL TIMES. COORDINATE WITH KING COUNTY METRO (CONSTRUCTION.COORD@KINGCOUNTY.GOV) A MINIMUM OF 10 CALENDAR DAYS IN ADVANCE OF IMPLEMENTING TEMPORARY TRAFFIC CONTROL THAT WOULD IMPACT A TRANSIT STOP. COORDINATE WITH KING COUNTY METRO REGARDING REQUIREMENTS FOR TEMPORARILY RELOCATING TRANSIT STOPS.
- A FLAGGER OPERATION SHALL BE REQUIRED TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS WHEN THE TRAFFIC SIGNAL OPERATIONS ARE IMPACTED AND/OR AS SHOWN IN THESE PLANS. THE TRAFFIC SIGNAL SHALL BE SET TO ALL RED "FLASH" MODE WHILE THE INTERSECTION IS BEING CONTROLLED BY FLAGGERS. WHEN FLAGGERS ARE USED AT SIGNALIZED INTERSECTIONS, APPROACHES INTO THE INTERSECTIONS SHALL BE REDUCED TO ONE LANE IN EACH DIRECTION.
- FLAGGING STATIONS SHALL BE ILLUMINATED DURING HOURS OF DARKNESS AS APPROVED BY THE ENGINEER. FLAGGERS SHOULD BE VISIBLE AND DISCERNABLE AS A FLAGGER FROM A MINIMUM DISTANCE OF 1,000'.
- MAINTAIN EXISTING LANE WIDTHS AT ALL TIMES ON SOUTH 288TH STREET. DEVICES SHALL BE OFFSET FROM LANE/EDGE LINES BY A MINIMUM OF 2'. THE TRAFFIC CONTROL SUPERVISOR MAY FIELD-ADJUST DEVICES UP TO THE LANE/EDGE LINE IF NEEDED, BUT DEVICES SHALL NOT ENCROACH INTO THE OPEN LANE. WHERE EXISTING LANE WIDTHS CANNOT FEASIBLY BE MAINTAINED, LANE WIDTHS SHALL BE 11' MINIMUM AT ALL TIMES, UNLESS OTHERWISE NOTED IN THESE PLANS.
- WHERE STEEL PLATES ARE NECESSARY TO COVER AN EXCAVATION WITHIN THE ROADWAY, EACH SIDE OF THE PLATE SHALL HAVE A MINIMUM 12" BEARING ON THE SIDES OF THE EXCAVATION. PLATES SHALL BE ANCHORED AND BEDDED ON TEMPORARY PAVEMENT PATCH MATERIAL, SHIMMED TO PREVENT MOVEMENT, ORIENTED PERPENDICULAR TO TRAFFIC, WITH BEDDING TAPERED ON ALL SIDES

- TO PROVIDE SMOOTH TRANSITION FOR ALL USERS. PLATES SHALL BE TEXTURED TO PROVIDE A NON-SKID SURFACE AS APPROVED BY THE ENGINEER. PLATES SHALL BE DELINEATED WITH PAINT AT ITS EDGES AND WARNING SIGN(S) W8-24 (STEEL PLATE AHEAD) INSTALLED AS DIRECTED BY THE ENGINEER.
- TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE CRASHWORTHY AND COMPLY WITH MUTCD PART 6.
 - CHANNELIZATION DEVICES USING DURING HOURS OF DARKNESS SHALL HAVE RETROREFLECTIVE BANDS.
 - EXISTING SIGNAGE IN CONFLICT WITH TEMPORARY TRAFFIC CONTROL SIGNAGE SHALL BE REMOVED OR SECURELY COVERED BY A METHOD APPROVED BY THE ENGINEER.
 - TEMPORARY TRAFFIC CONTROL SIGNS IN PLACE AT ONE LOCATION CONTINUOUSLY FOR LONGER THAN THREE DAYS SHALL BE CLASS A.
 - CLASS A CONSTRUCTION SIGNING SHALL BE INSTALLED AS SHOWN IN THESE PLANS AND PER WSDOT STANDARD PLANS K-80.10 AND G-22.10.
 - THE CONTRACTOR SHALL FIELD STAKE AND VERIFY FINAL LOCATIONS OF PROPOSED CLASS A SIGNS WITH THE ENGINEER PRIOR TO INSTALLATION.
 - SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND, UNLESS OTHERWISE NOTED IN THESE PLANS.
 - ROAD WORK AHEAD (W20-1) SIGNS SHALL BE PLACED ON EVERY MINOR APPROACH WITHIN THE PROJECT SITE.
 - DIAMOND SHAPED WARNING SIGNS (W SERIES) SHALL BE 48"x48", UNLESS OTHERWISE NOTED IN THESE PLANS.
 - SIGNS MOUNTED TO TYPE 3 BARRICADES SHALL BE INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS.
 - TEMPORARY CONCRETE BARRIERS SHALL BE INSTALLED PER WSDOT STANDARD PLAN K-80.35. TEMPORARY CONCRETE BARRIERS MAY BE PLACED UNANCHORED, UNLESS OTHERWISE NOTED IN THESE PLANS.
 - TEMPORARY NARROW CONCRETE BARRIERS SHALL BE INSTALLED PER WSDOT STANDARD PLAN K-80.37.
 - PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) BOARDS SHOULD BE LOCATED BEHIND GUARDRAIL OR BARRIER WHEREVER FEASIBLE. PCMS BOARDS NOT LOCATED BEHIND GUARDRAIL OR BARRIER SHALL HAVE A TAPER CONSISTING A MINIMUM OF 3 CHANNELIZATION DEVICES IN ADVANCE OF THE PCMS BOARD. PCMS BOARDS SHALL BE REMOVED WHEN NOT DISPLAYING ANY MESSAGES.
 - TYPE 3 BARRICADES SHALL BE INSTALLED PER MUTCD CHAPTER 6F SECTION 6F.63.
 - PROVIDE PEDESTRIAN AND BICYCLE TRAFFIC WITH AN ALTERNATE ROUTE WHEN EXISTING FACILITIES ARE INTERRUPTED BY CONSTRUCTION ACTIVITIES. ALTERNATIVE ROUTES SHALL BE CLEARLY DELINEATED AND SEPARATED FROM THE CONSTRUCTION ACTIVITIES. THE FOLLOWING GUIDANCE IS PROVIDED REGARDING TEMPORARY TRAFFIC CONTROL FOR PEDESTRIANS:
 - TEMPORARY TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION WORK/MATERIALS/EQUIPMENT SHALL NOT ENCROACH INTO THE USABLE WIDTH OF PEDESTRIAN ROUTES.
 - PROVIDE A MINIMUM 84" UNOBSTRUCTED VERTICAL CLEARANCE FOR PEDESTRIAN ROUTES. SIGNS AND OTHER DEVICES LOWER THAN 84" ABOVE THE PEDESTRIAN ROUTE SHALL NOT PROJECT GREATER THAN 4" INTO THE ROUTE.
 - MAINTAIN THE WIDTH OF EXISTING PEDESTRIAN ROUTES WHENEVER FEASIBLE. WHEN NOT FEASIBLE TO MAINTAIN A MINIMUM WIDTH OF 60" THROUGH THE LENGTH OF THE ROUTE, MAINTAIN A MINIMUM WIDTH OF 48" THROUGH THE LENGTH OF THE ROUTE AND PROVIDE 60"x60" PASSING ZONES SPACED AT MAXIMUM INTERVALS OF 200' ALONG THE ROUTE.
 - PROVIDE A SMOOTH, CONTINUOUS, HARD SURFACE THROUGH THE LENGTH AND WIDTH OF THE PEDESTRIAN ROUTE. THERE SHALL BE NO CURBS OR VERTICAL ELEVATION CHANGES GREATER THAN 1/2" IN GRADE OR TERRAIN. VERTICAL ELEVATION DIFFERENCES BETWEEN 1/4" AND 1/2" SHALL BE BEVELED AT A MAXIMUM SLOPE OF 2:1.
 - IF CHANNELIZATION DEVICES ARE USED TO DELINEATE A PEDESTRIAN ROUTE, A CONTINUOUS DETECTABLE EDGING SHALL BE PROVIDED THROUGH THE LENGTH OF THE ROUTE. THE TOP OF THE EDGING SHALL BE A MINIMUM OF 32" ABOVE THE ROUTE SURFACE AND THE BOTTOM OF THE EDGING SHALL BE A MAXIMUM OF 2" ABOVE THE ROUTE SURFACE.
 - AT LOCATIONS WHERE PROVIDING AN ALTERNATE PEDESTRIAN ROUTE IS INFEASIBLE, PEDESTRIAN CLOSURE/DIRECTION SIGNAGE SHALL BE POSTED IN ADVANCE OF THE PEDESTRIAN FACILITY CLOSURE AT THE NEAREST

- MARKED CROSSWALK OR INTERSECTION.
- PHYSICAL BARRICADES SHALL BE INSTALLED WHEREVER THE EXISTING ACCESSIBLE ROUTE IS CLOSED TO PREVENT PEDESTRIANS FROM INADVERTENTLY ENTERING A WORK AREA.
 - TEMPORARY RAMPS SHALL BE CONSTRUCTED WHEREVER AN ALTERNATE PEDESTRIAN ROUTE CROSSES A CURB AND EXISTING CURB RAMPS ARE UNAVAILABLE. THE MINIMUM WIDTH OF THE RAMP SHALL BE 48"; THE MAXIMUM SLOPE OF THE RAMP SHALL BE 8.3%; THE MAXIMUM CROSS SLOPE OF THE RAMP SHALL BE 2%. THE BOTTOM OF THE TEMPORARY RAMP SHALL BE FLUSH WITH THE ROADWAY. TEMPORARY DETECTABLE WARNING MATS (OR SIMILAR) SHALL BE INSTALLED AT STREET CROSSINGS.
 - INFORMATION REGARDING CLOSED PEDESTRIAN ROUTES, ALTERNATE CROSSINGS, AND SIGN/SIGNAL INFORMATION SHALL BE COMMUNICATED TO PEDESTRIANS BY PROVIDING DEVICES SUCH AS AUDIBLE INFORMATION DEVICES, ACCESSIBLE PEDESTRIAN SIGNALS, AND/OR DETECTABLE BARRIER/CHANNELIZATION DEVICES.
28. TRAFFIC CONTROL PLANS WERE PREPARED AS SUPPLEMENTAL DOCUMENTATION FOR CONSTRUCTION OF MOST FEATURES IN THE PLAN SET. THE PLANS IN THE BID PACKAGE MAY NOT CONSTITUTE THE ENTIRETY OF THE TRAFFIC CONTROL PLANS REQUIRED FOR THE CONTRACTOR TO CONSTRUCT THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR SUBMITTING ALL NECESSARY TRAFFIC CONTROL PLANS TO CONSTRUCT THE PROJECT TO THE CITY OF FEDERAL WAY.

SIGN SPACING = X (1)		
RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ±
URBAN STREETS	25 MPH OR LESS	100' ±

LEGEND

DESCRIPTION	SYMBOL
RIGHT-OF-WAY	— — — — —
SIGN NOTE	#
CHANNELIZATION DEVICE (HIGH VISIBILITY)	⊙
TEMPORARY SIGN ON PORTABLE CRASHWORTHY SUPPORT	K
DIRECTION OF TRAVEL	←
PROTECTIVE VEHICLE	☐
WORK AREA	▨
SEQUENTIAL ARROW PANEL	⇒⇒
TRAFFIC CONTROL FLAGGER	♠
TRANSPORTABLE ATTENUATOR	☐

BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (FT)	155	200	250	305	360	425	495	570	645	730
TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R										
HOST VEHICLE WEIGHT 9,900 TO 22,000 LBS					HOST VEHICLE WEIGHT > 22,000 LBS					
< 45 MPH	45-55 MPH	> 55 MPH	< 45 MPH	45-55 MPH	> 55 MPH					
100'	123'	172'	74'	100'	150'					
PROTECTIVE VEHICLE (WORK VEHICLE) = R										
NO SPECIFIED DISTANCE REQUIRED										

MINIMUM LANE CLOSURE TAPER LENGTH = L (FT)										
LANE WIDTH (FT)	POSTED SPEED (MPH)									
	20	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (FT)										
SHOULDER WIDTH (FT)	POSTED SPEED (MPH)									
	25	30	35	40	45	50	55	60	65	70
8	40	40	60	90	120	130	150	160	170	190
10	40	60	90	90	150	170	190	200	220	240
USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.										

CHANNELIZATION DEVICE SPACING (FT)		
MPH	TAPER	TANGENT
50 / 65	40	80
35 / 45	30	60
25 / 30	20	40



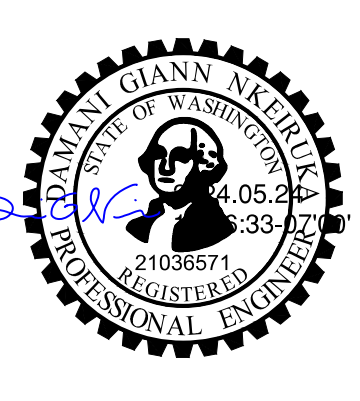
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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC CONTROL PLAN - NOTES

CITY PROJECT #:
36238

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OF **81**

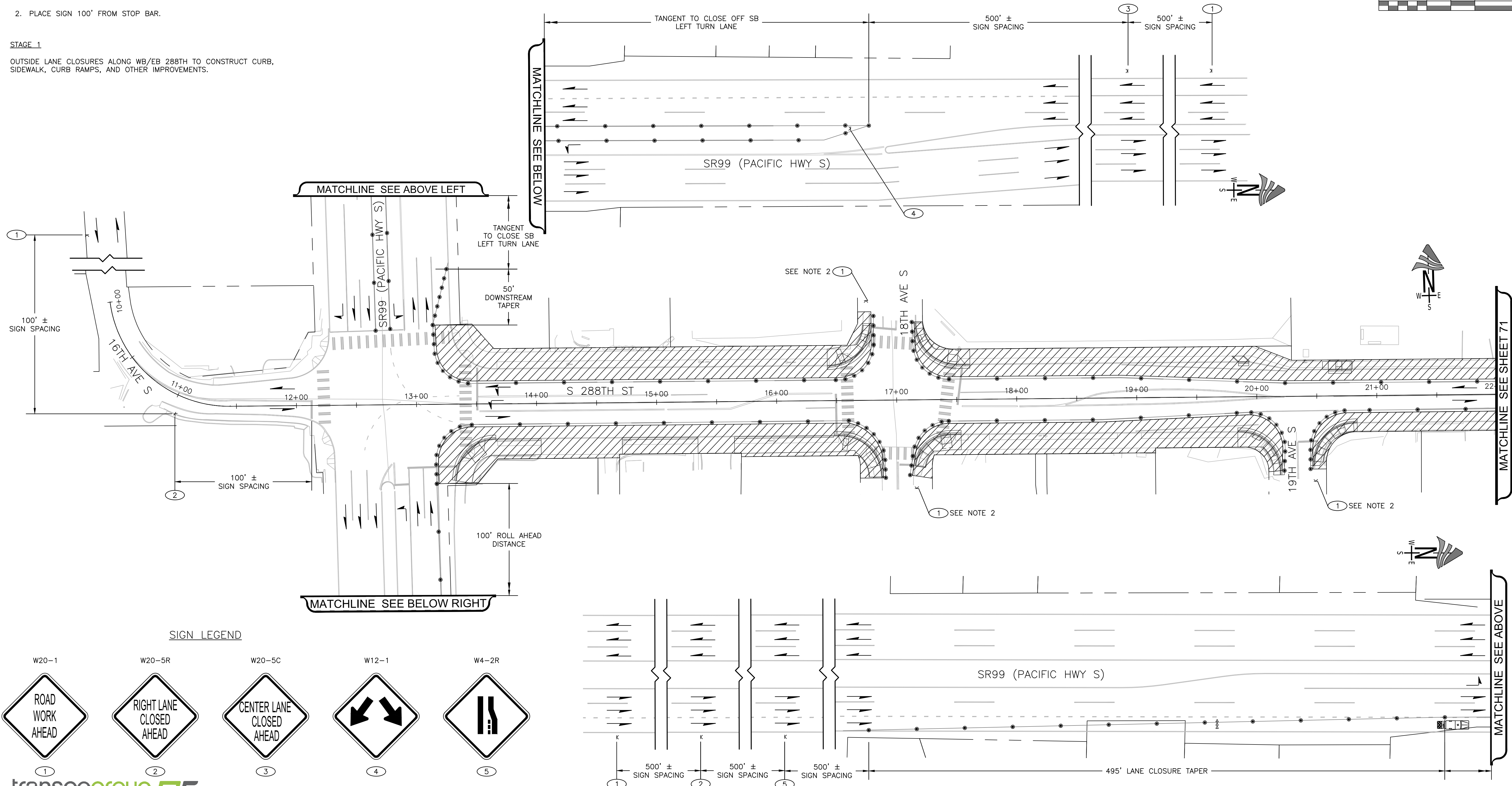
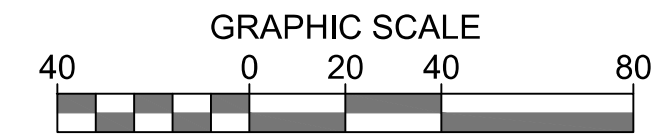
NOTES:

- SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
- PLACE SIGN 100' FROM STOP BAR.

STAGE 1

OUTSIDE LANE CLOSURES ALONG WB/EB 288TH TO CONSTRUCT CURB, SIDEWALK, CURB RAMPS, AND OTHER IMPROVEMENTS.

SEC.32/33 T.22N. R.4E. W.M.
SEC.4/5 T.21N. R.4E. W.M.



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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC CONTROL PLAN - STAGE 1

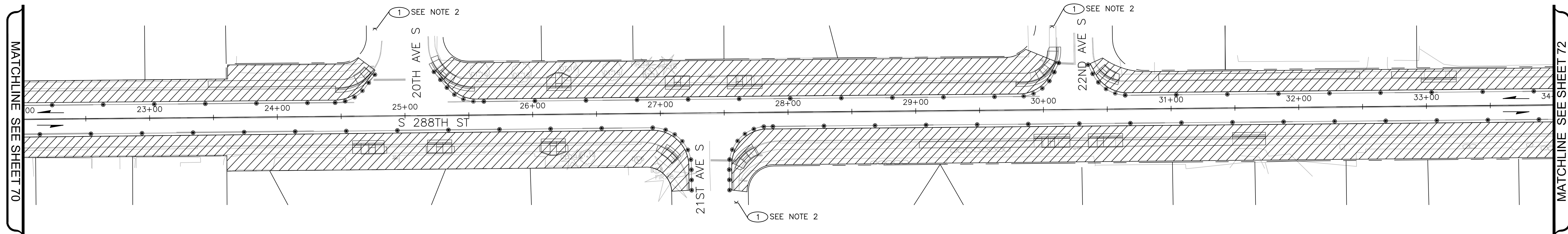
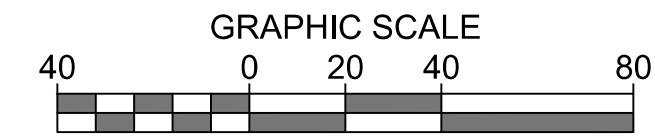
CITY PROJECT #: 36238
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OF 81

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NOTES:

1. SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
2. PLACE SIGN 100' FROM STOP BAR.

SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



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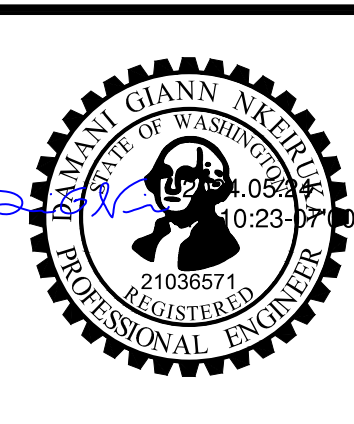


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TRAFFIC CONTROL PLAN - STAGE 1

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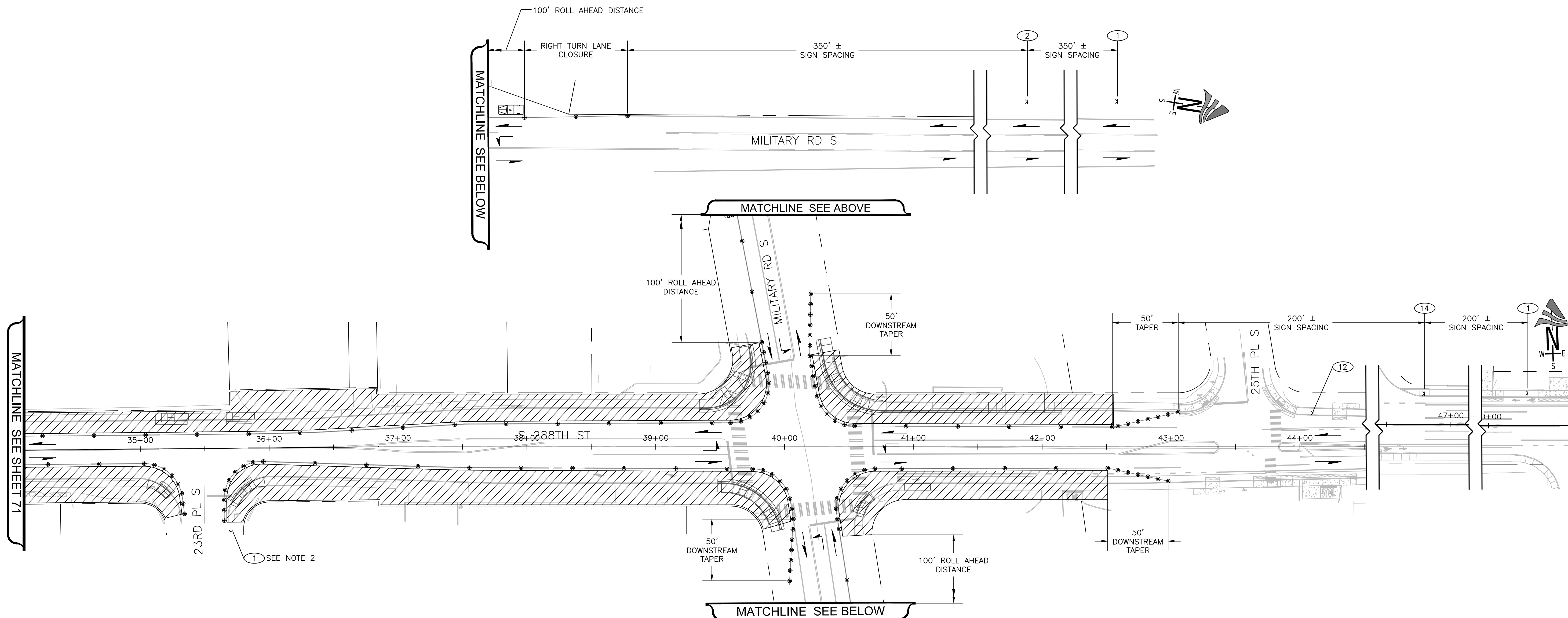
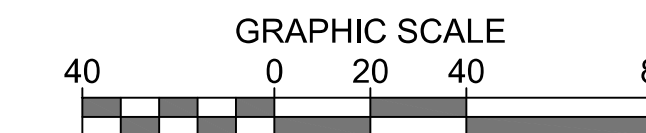
SHT. **71**
OF **81**

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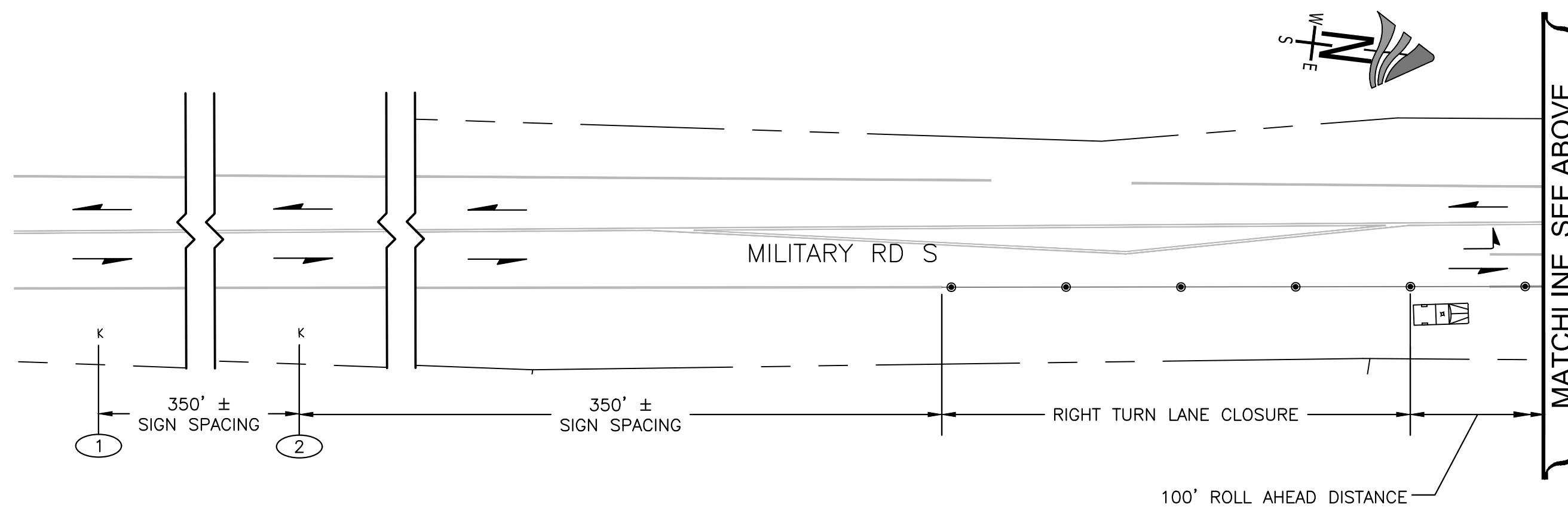
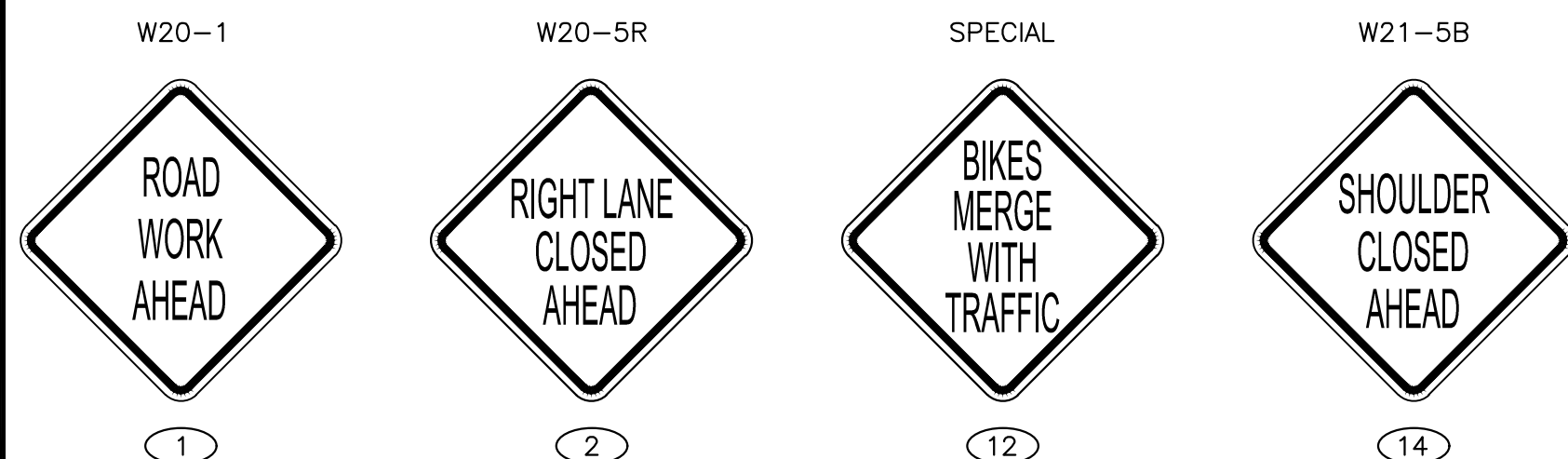
NOTES:

1. SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
2. PLACE SIGN 100' FROM STOP BAR.

SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



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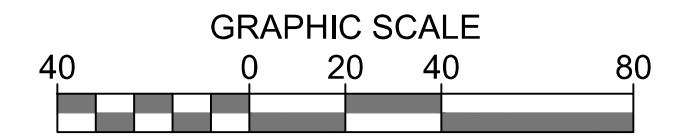
S 288TH ST ROAD DIET - PHASE 2
TRAFFIC CONTROL PLAN - STAGE 1

CITY PROJECT #:
36238
SHT. **72**
OF **81**

NOTES:

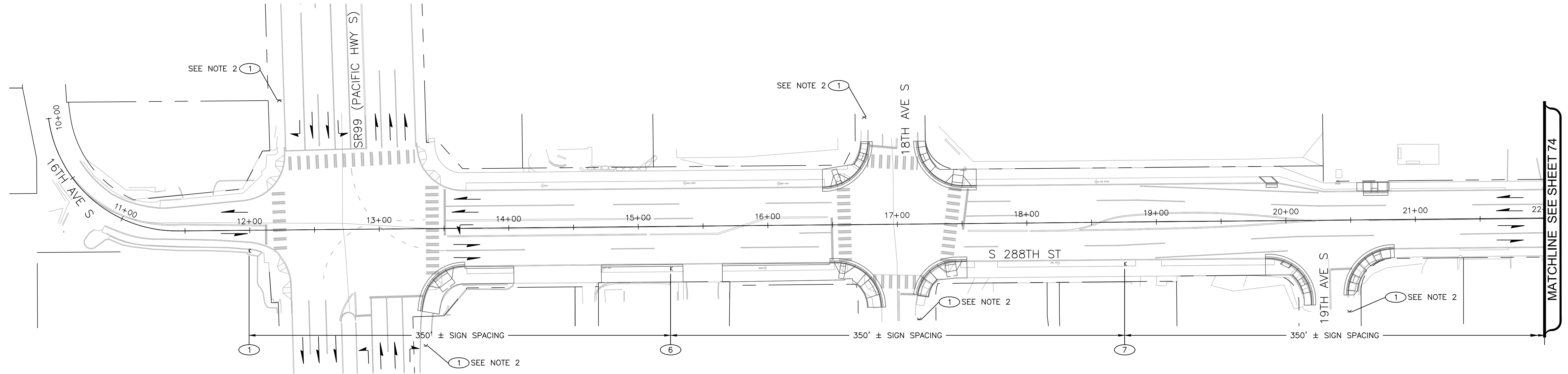
- SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
- PLACE SIGN 100' FROM STOP BAR.

SEC.32/33 T.22N. R.4E. W.M.
SEC.4/5 T.21N. R.4E. W.M.

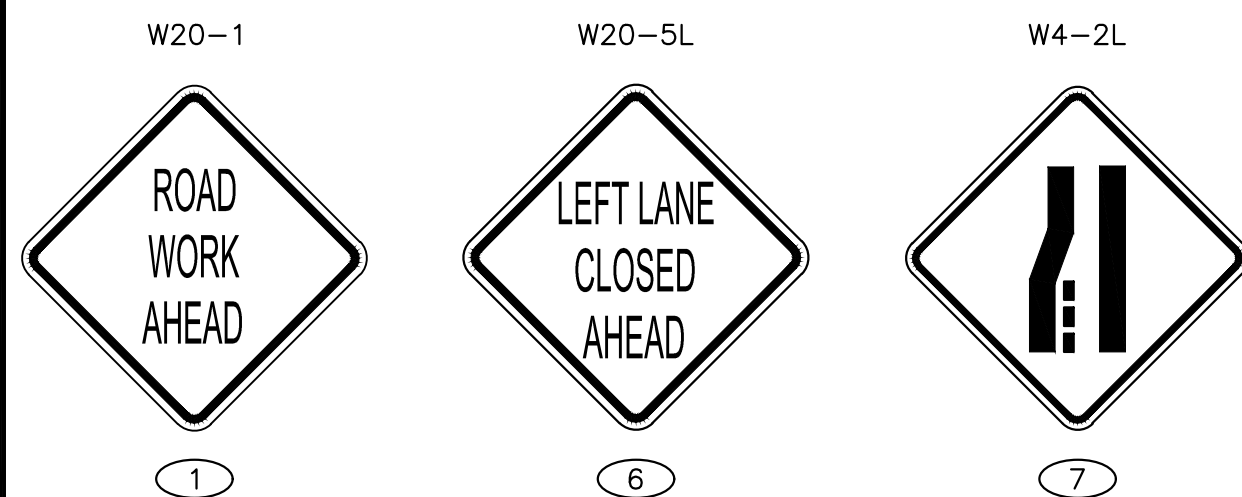


STAGE 2

INSIDE LANE CLOSURES ALONG WB/EB 288TH TO CONSTRUCT PEDESTRIAN REFUGE ISLAND.



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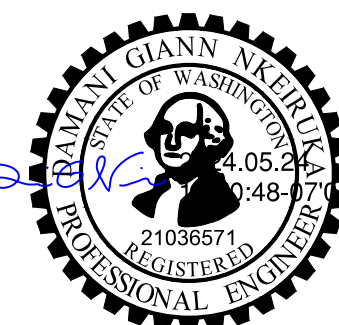
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TRAFFIC CONTROL PLAN - STAGE 2

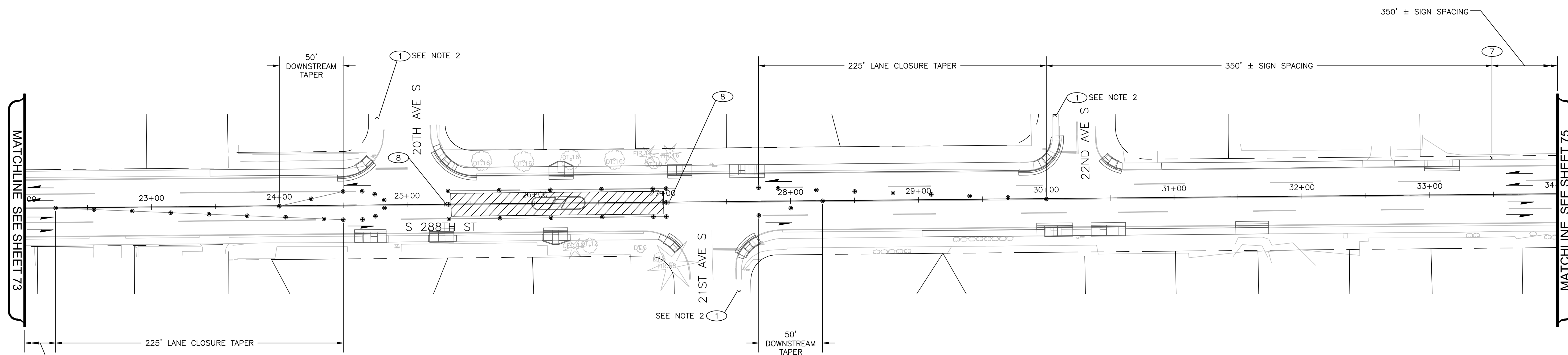
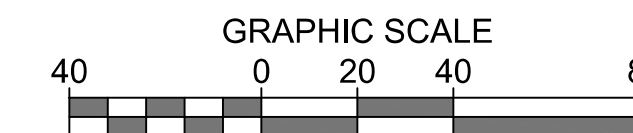
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OF 81

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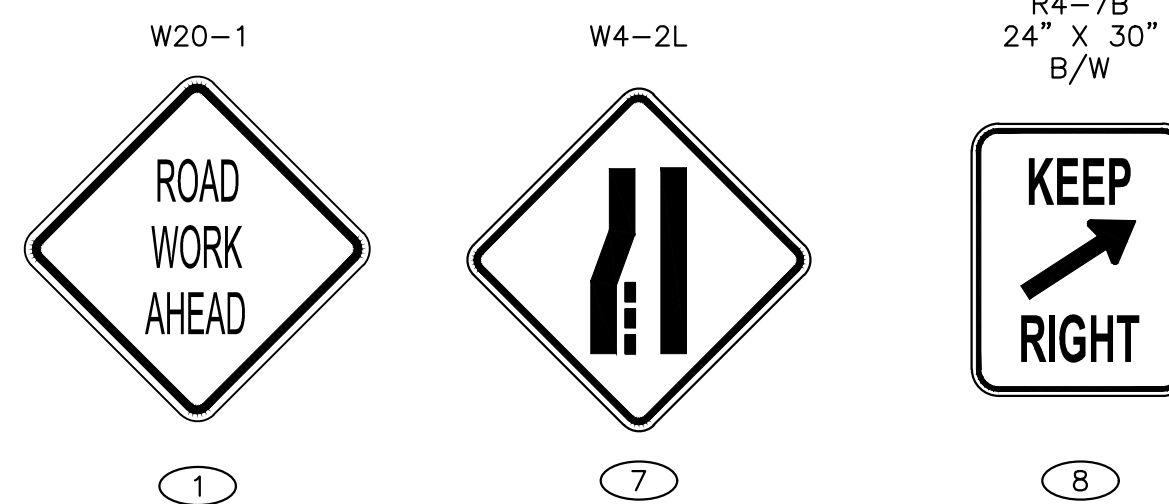
NOTES:

1. SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
2. PLACE SIGN 100' FROM STOP BAR.

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TRAFFIC CONTROL PLAN - STAGE 2

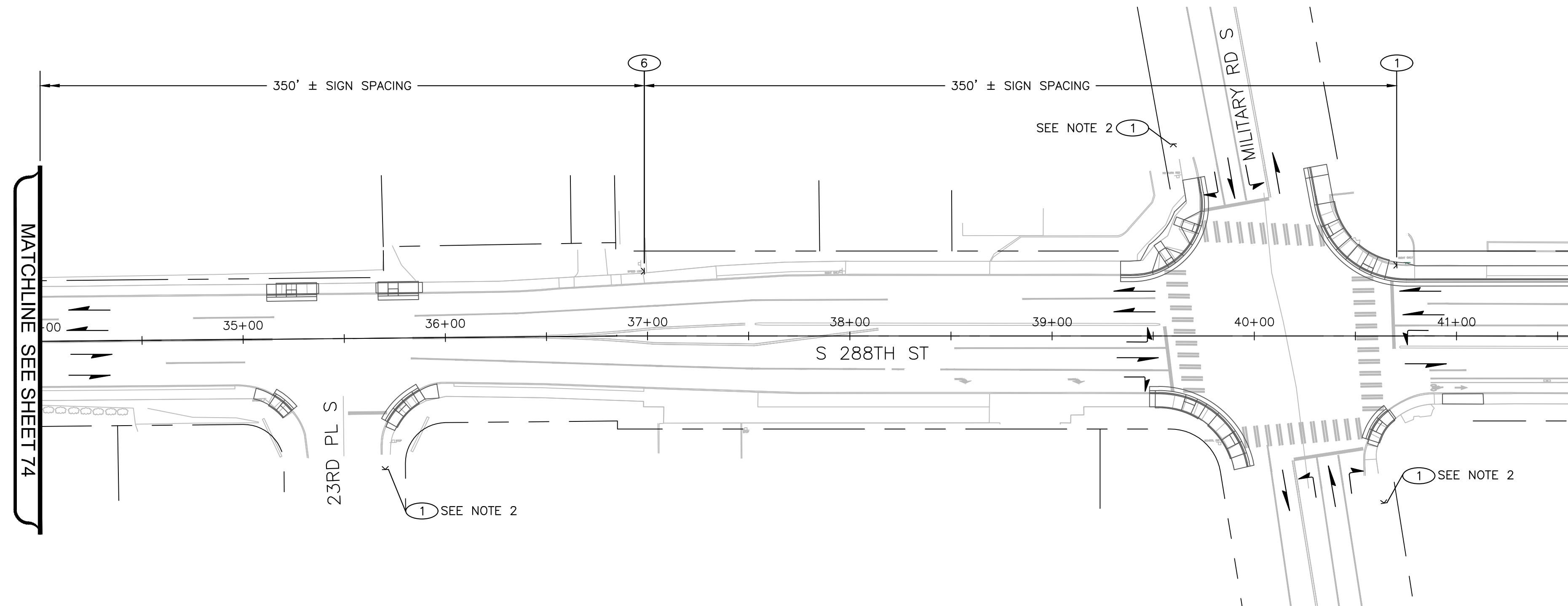
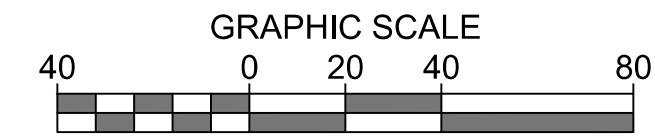
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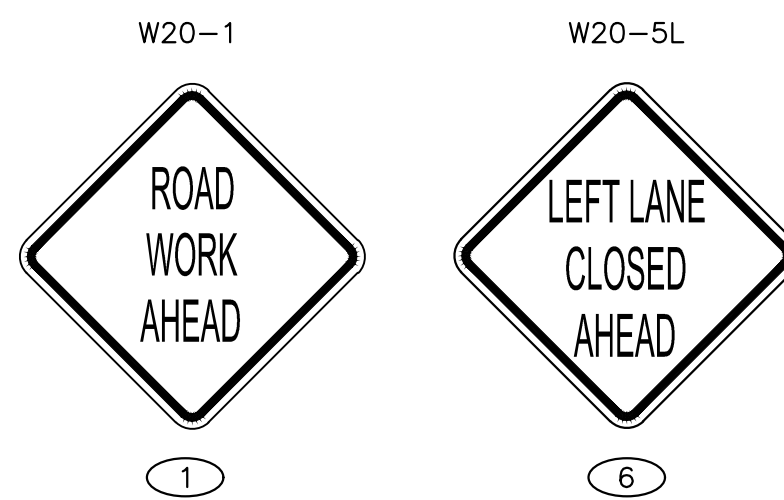
NOTES:

1. SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
2. PLACE SIGN 100' FROM STOP BAR.

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SEC.4 T.21N. R.4E. W.M.



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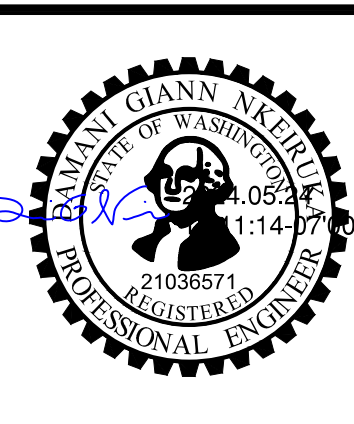


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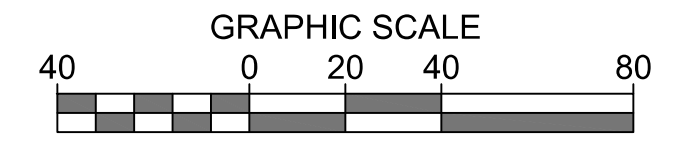
TRAFFIC CONTROL PLAN - STAGE 2

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OF **81**

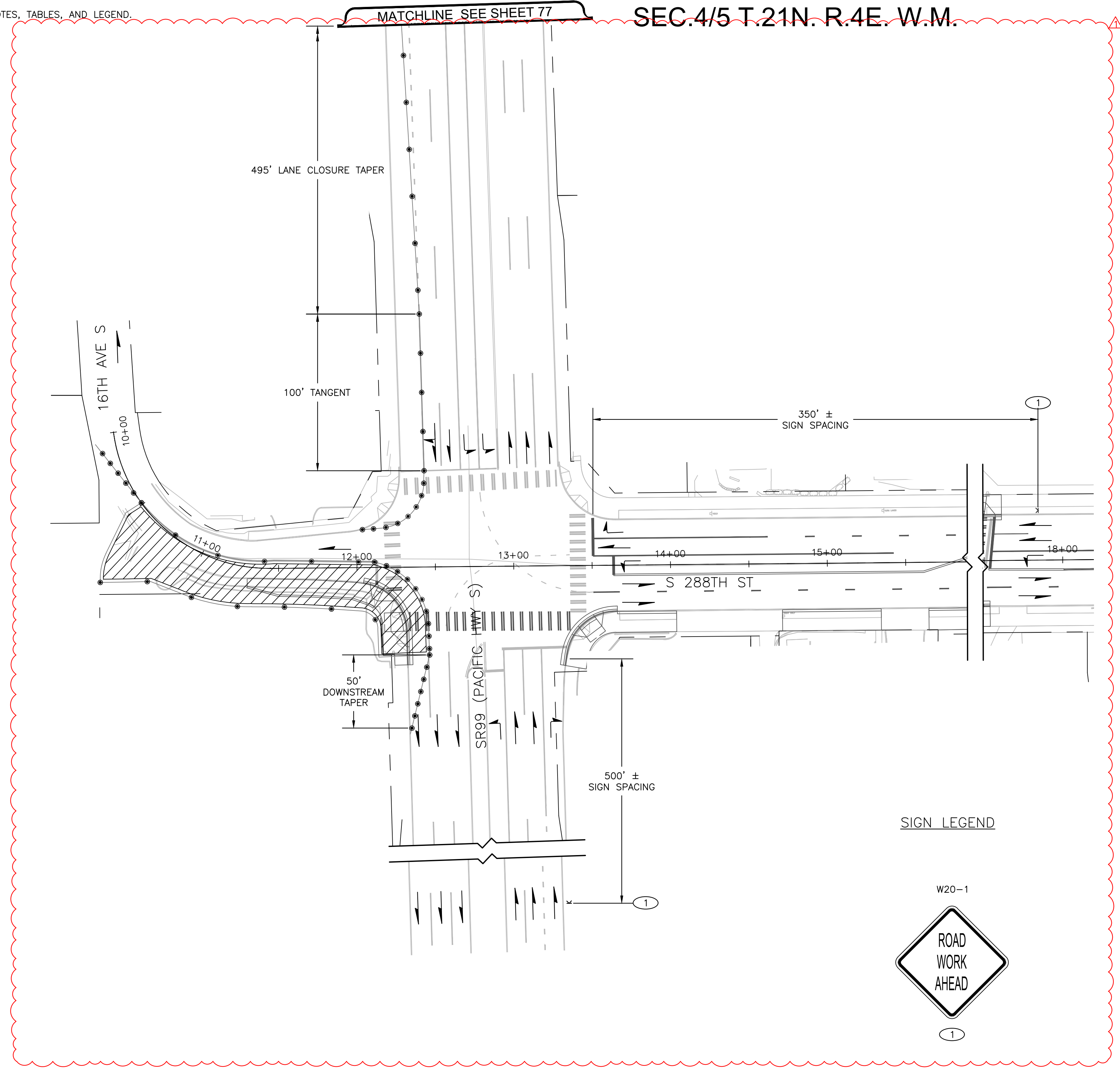
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SEC.32/33 T.22N. R.4E. W.M.
SEC.4/5 T.21N. R.4E. W.M.



NOTES:

1. SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
2. PLACE SIGN 100' FROM STOP BAR.



STAGE 3
WB TRAFFIC ONLY ALONG 16TH AVE TO CONSTRUCT CURB, SIDEWALK, CURB RAMP, AND OTHER IMPROVEMENTS. DETOUR PLAN FOR EB TRAFFIC APPROACHING SR99 IS NOT SHOWN IN THESE PLANS AND WILL BE PREPARED BY OTHERS.

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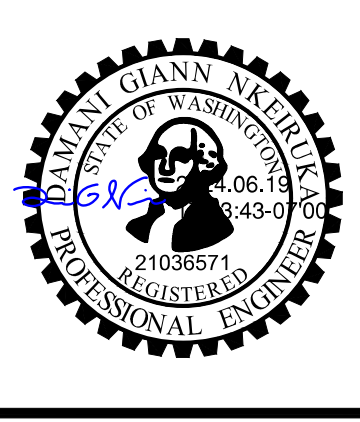


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S 288TH ST ROAD DIET - PHASE 2

TRAFFIC CONTROL PLAN - STAGE 3

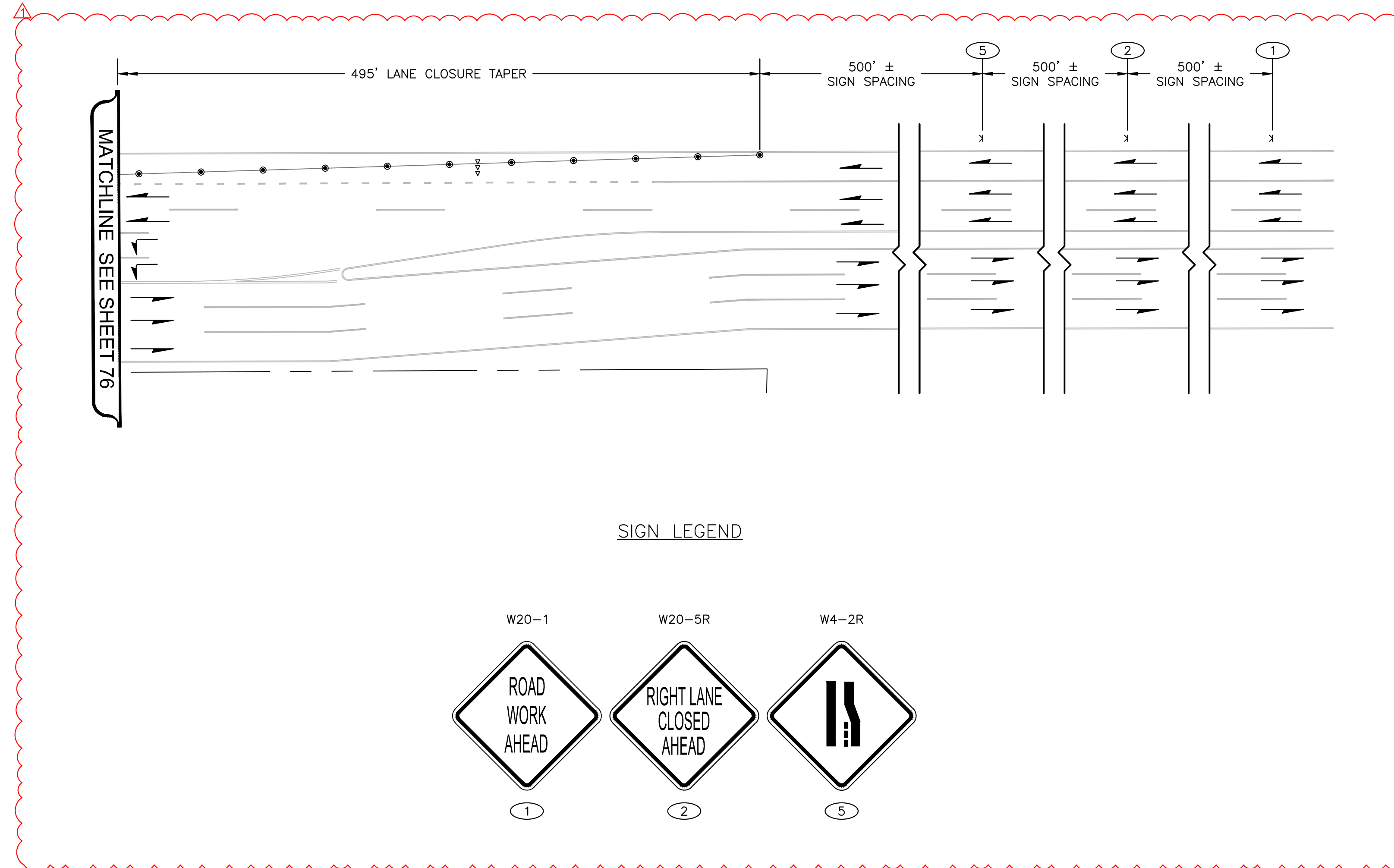
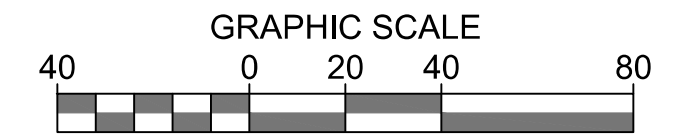
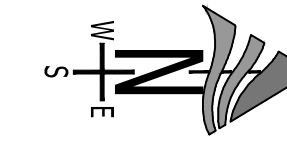
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NOTES:

1. SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
2. PLACE SIGN 100' FROM STOP BAR.

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SEC.4/5 T.21N. R.4E. W.M.



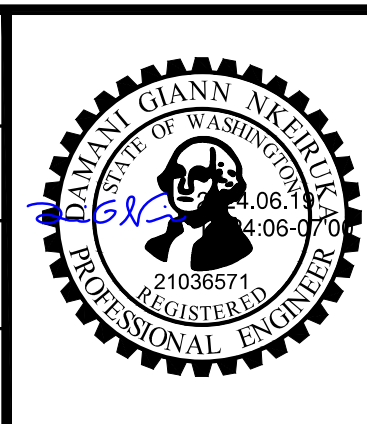
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TRAFFIC CONTROL PLAN - STAGE 3

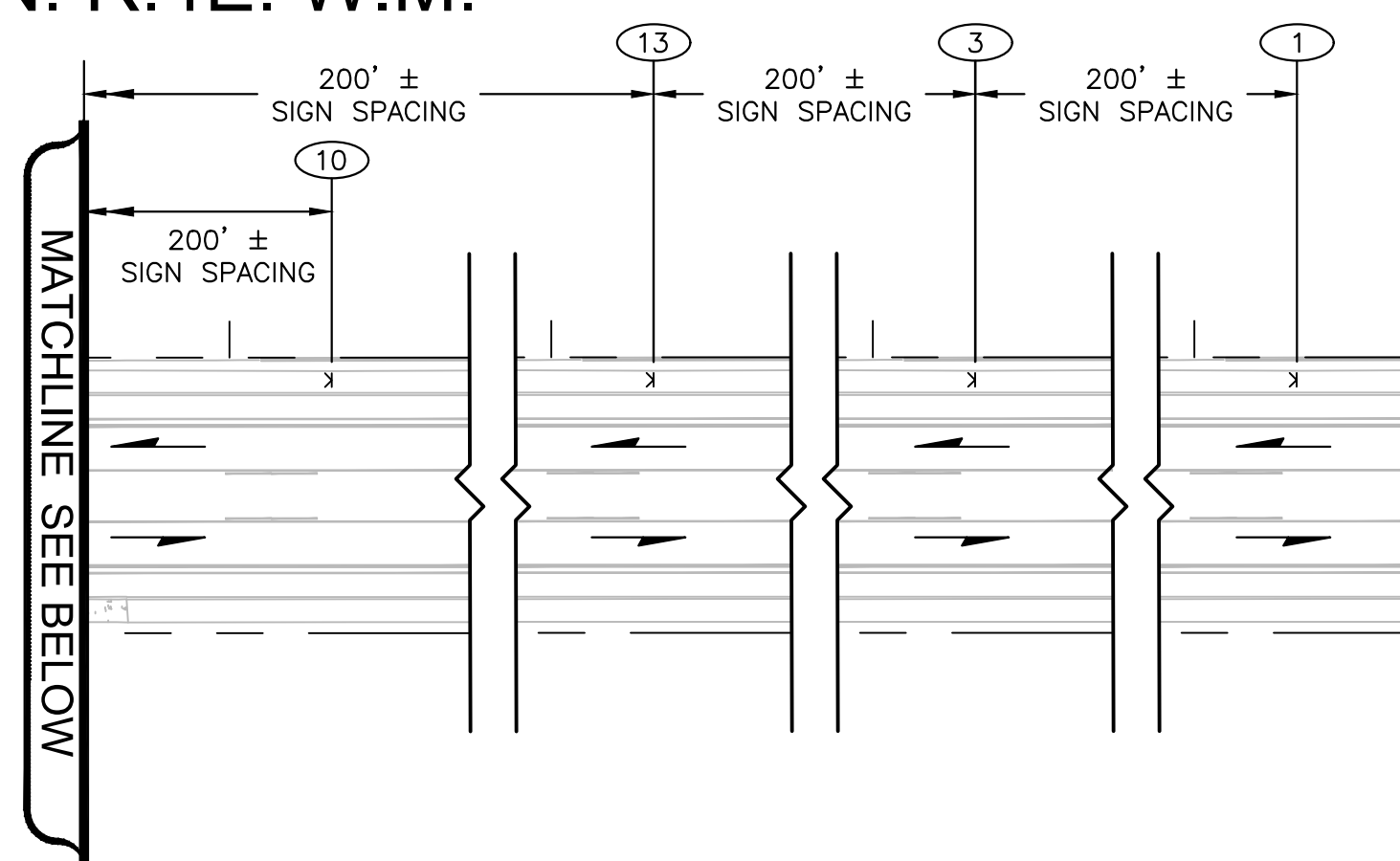
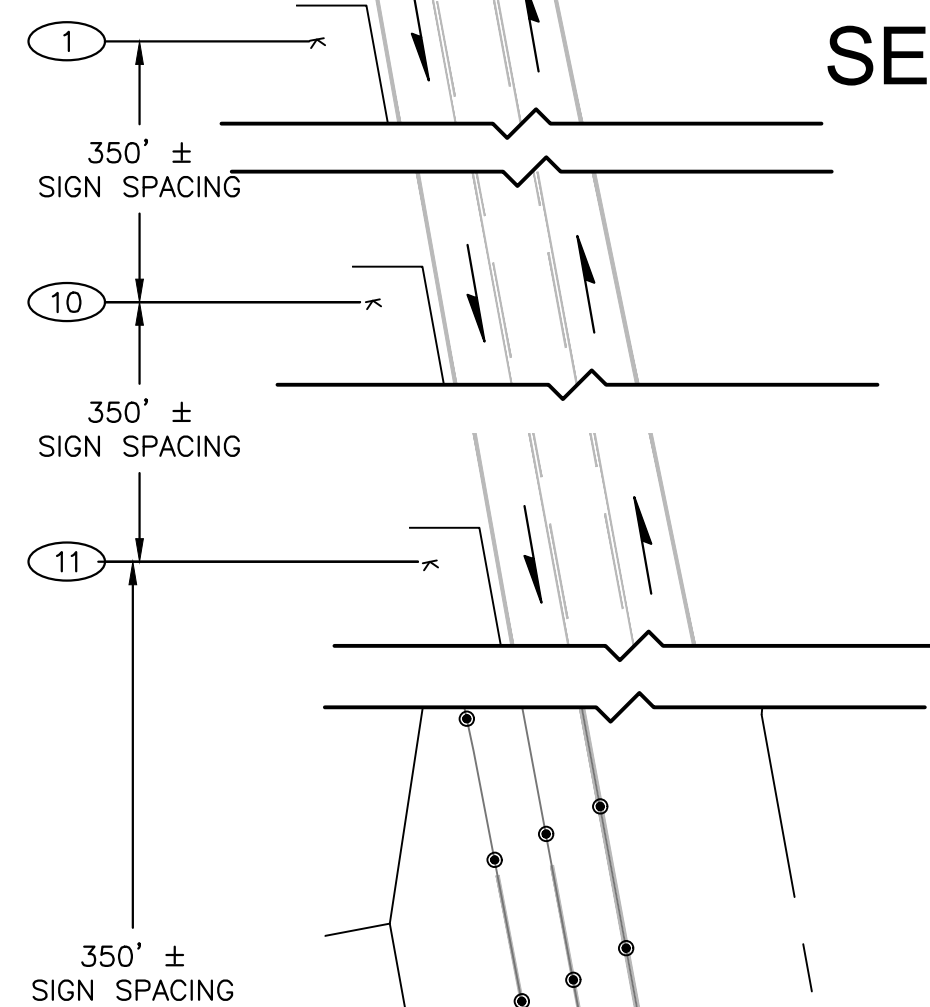
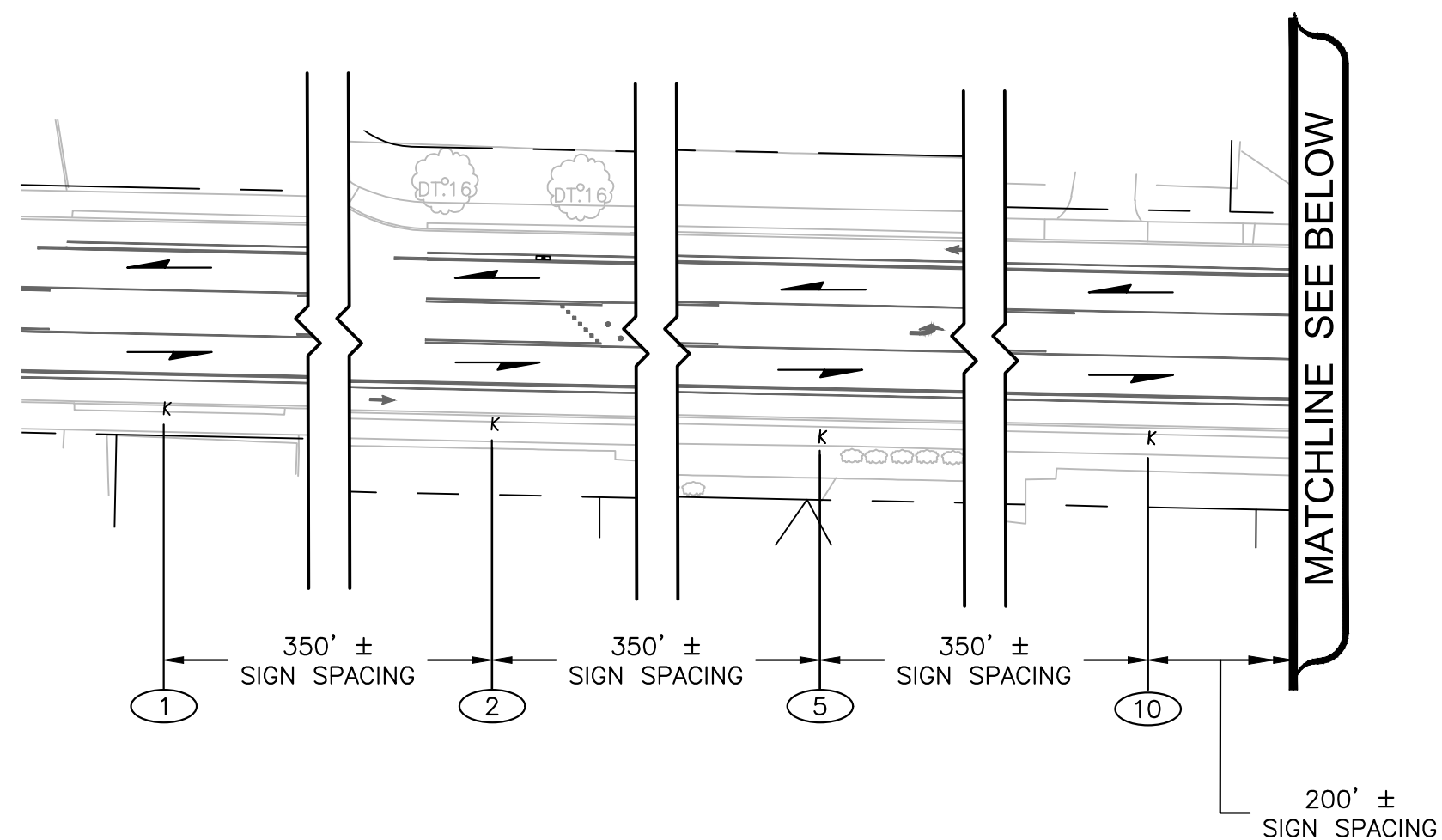
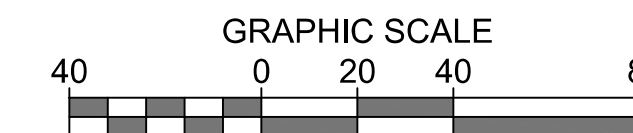
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OF	81

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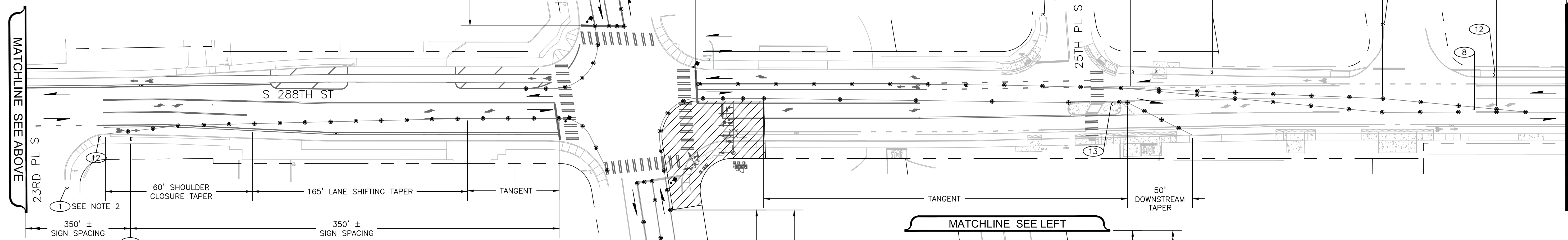
- SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
- PLACE SIGN 100' FROM STOP BAR.

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SEC.4 T.21N. R.4E. W.M.

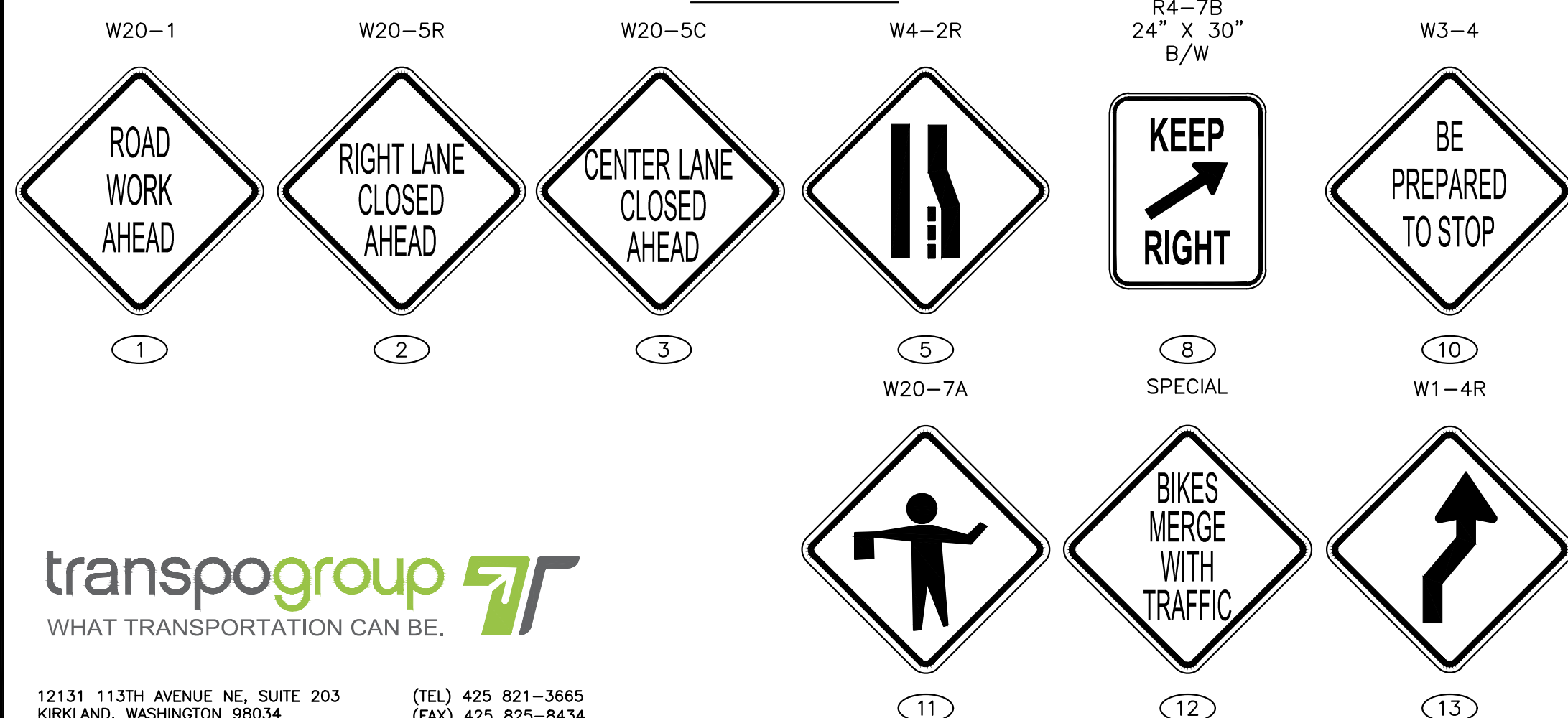


STAGE 4A

FLAGGER OPERATION FOR MAST ARM ERECTION ON SE CORNER. ALL APPROACHING LEGS INTO THE INTERSECTION SHALL REDUCE TO SINGLE LANE APPROACH IN EACH DIRECTION FOR THE FLAGGER OPERATION.



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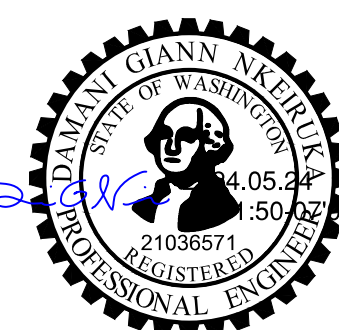
TRAFFIC CONTROL PLAN - STAGE 4A

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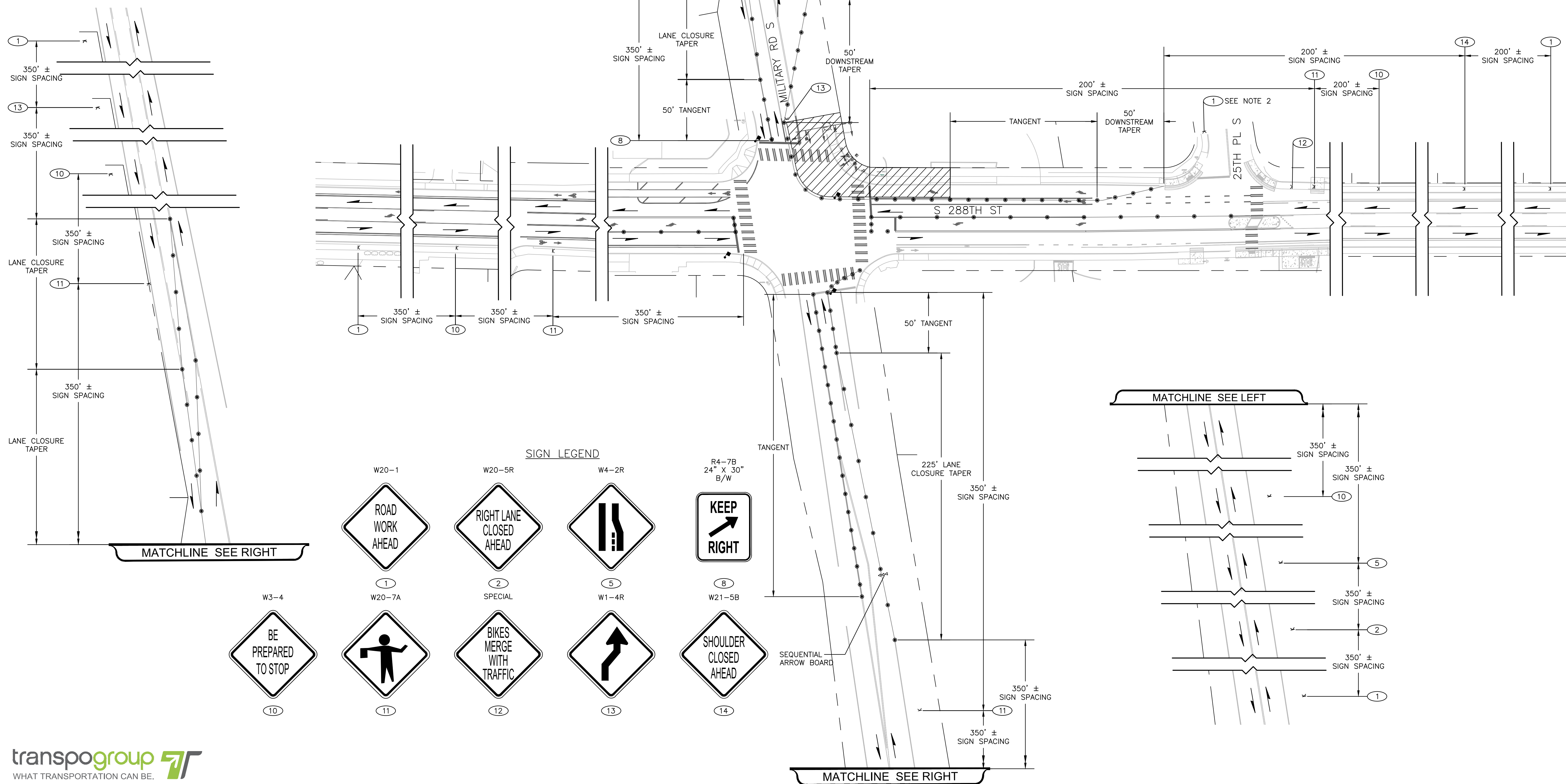
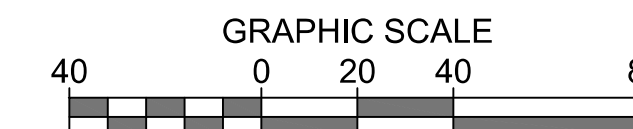
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- PLACE SIGN 100' FROM STOP BAR.

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SEC.4 T.21N. R.4E. W.M.

STAGE 4B

FLAGGER OPERATION FOR MAST ARM ERECTION ON NE CORNER. ALL APPROACHING LEGS INTO THE INTERSECTION SHALL REDUCE TO SINGLE LANE APPROACH IN EACH DIRECTION FOR THE FLAGGER OPERATION.



SIGN LEGEND

W20-1 1	W20-5R 2	W4-2R 5	R4-7B 24" x 30" B/W 8
W3-4 10	W20-7A 11	SPECIAL 12	W1-4R 13
			W21-5B 14



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TRAFFIC CONTROL PLAN - STAGE 4B

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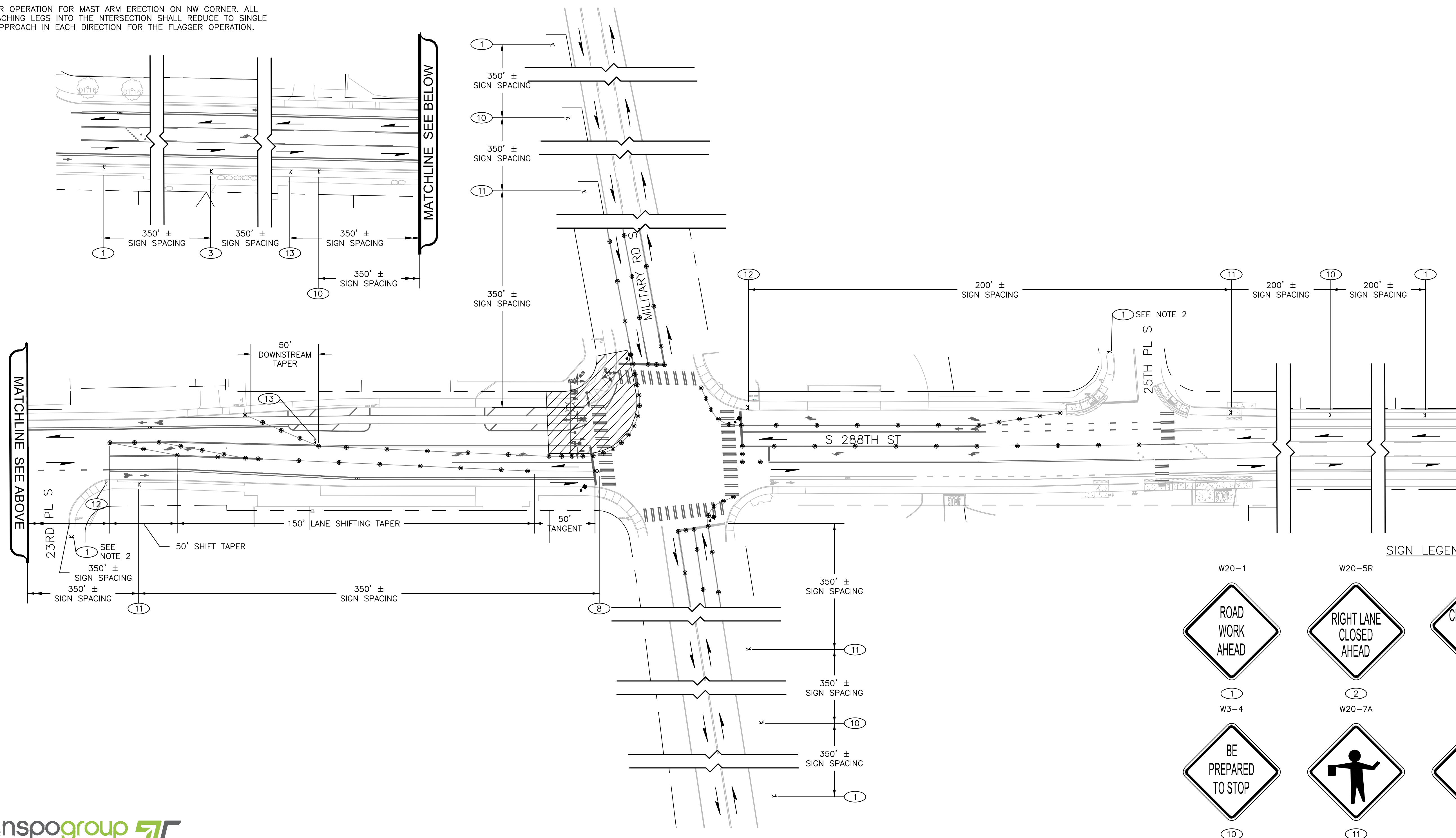
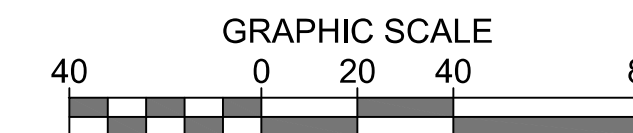
NOTES:

- SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
- PLACE SIGN 100' FROM STOP BAR.

STAGE 4C

FLAGGER OPERATION FOR MOST ARM ERECTION ON NW CORNER. ALL APPROACHING LEGS INTO THE INTERSECTION SHALL REDUCE TO SINGLE LANE APPROACH IN EACH DIRECTION FOR THE FLAGGER OPERATION.

SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



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W20-1 1	W20-5R 2	W20-5C 3	R4-7B 24" X 30" B/W 8
W3-4 10	W20-7A 11	SPECIAL 12	W1-4R 13



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TRAFFIC CONTROL PLAN - STAGE 4C

CITY PROJECT #: 36238

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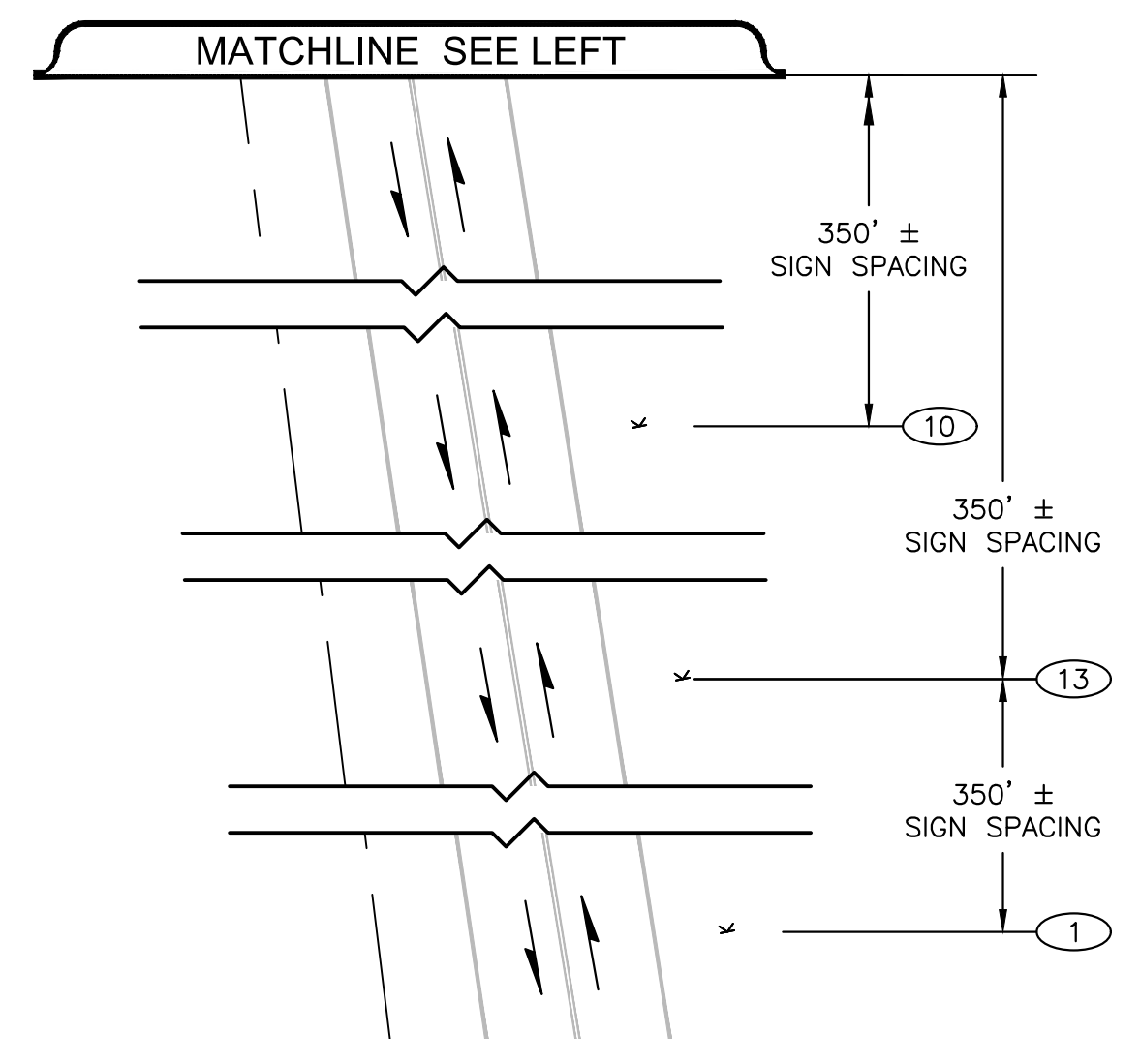
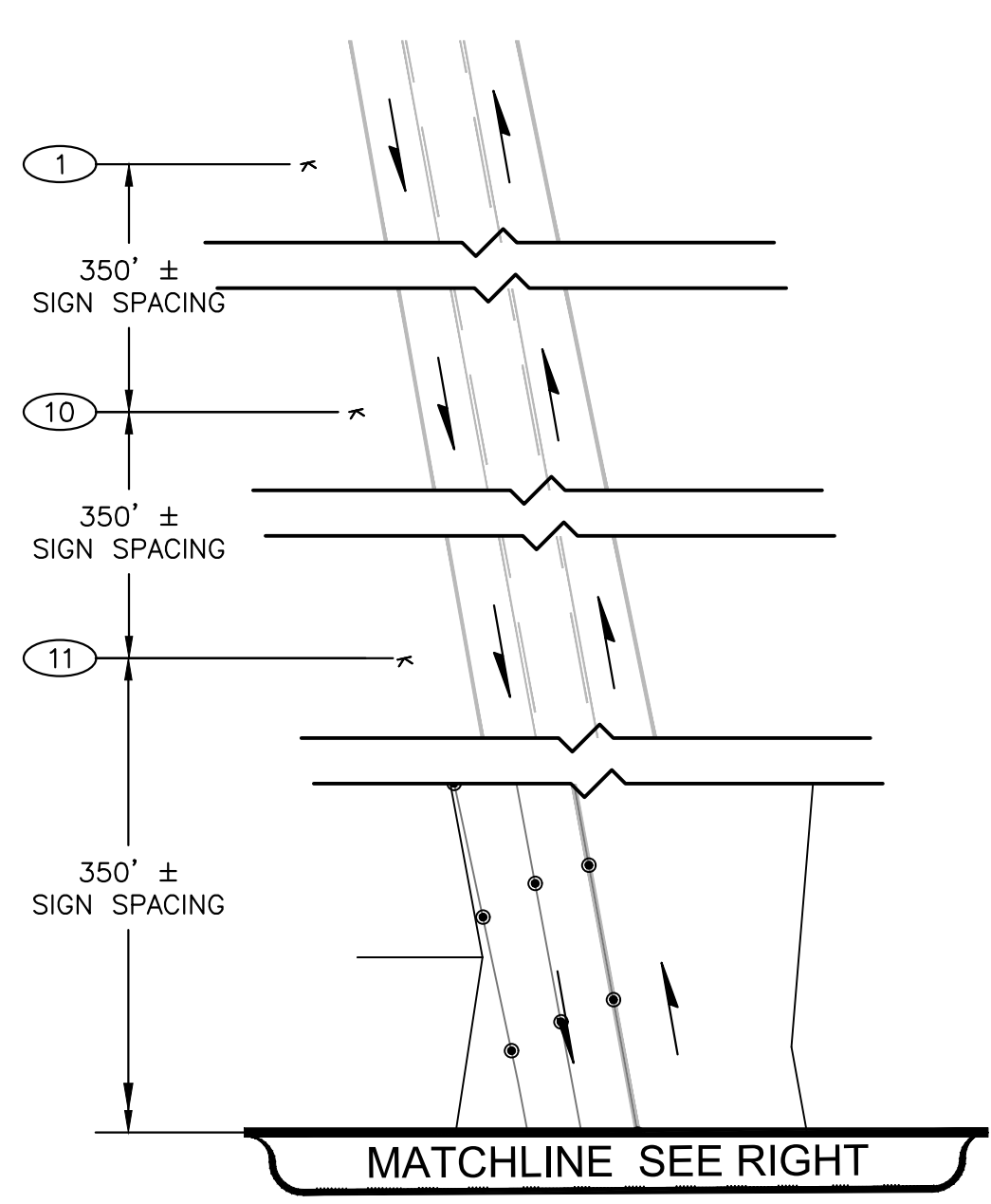
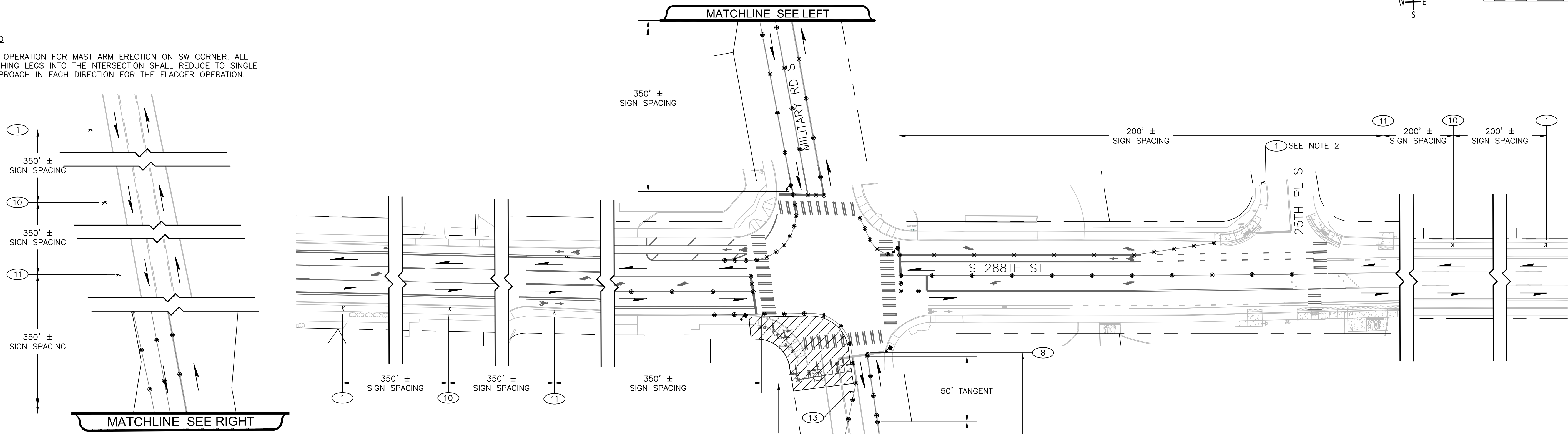
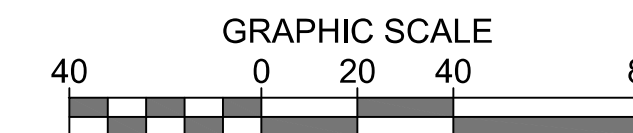
NOTES:

- SEE SHEET 69 FOR TRAFFIC CONTROL GENERAL NOTES, TABLES, AND LEGEND.
- PLACE SIGN 100' FROM STOP BAR.

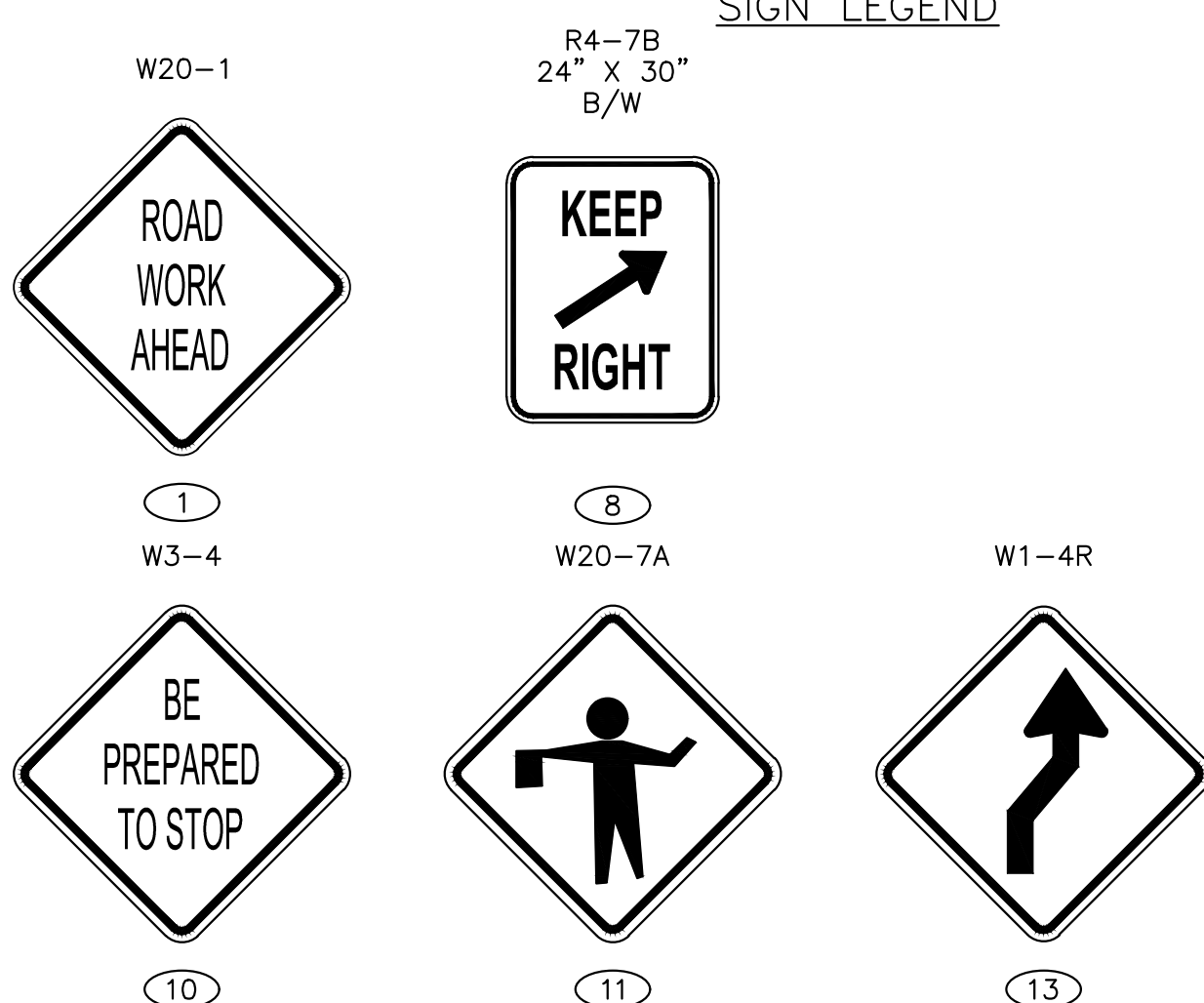
STAGE 4D

FLAGGER OPERATION FOR MAST ARM ERECTION ON SW CORNER. ALL APPROACHING LEGS INTO THE INTERSECTION SHALL REDUCE TO SINGLE LANE APPROACH IN EACH DIRECTION FOR THE FLAGGER OPERATION.

SEC.33 T.22N. R.4E. W.M.
SEC.4 T.21N. R.4E. W.M.



SIGN LEGEND

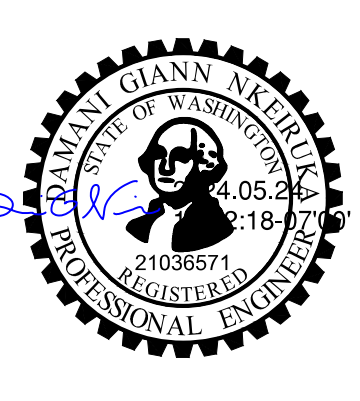


12131 113TH AVENUE NE, SUITE 203
KIRKLAND, WASHINGTON 98034
(TEL) 425 821-3665
(FAX) 425 825-8434

UNOFFICIAL COPY Official bid documents, plan holder's list, and addenda (if applicable) are available on BXVA.com

33325 8TH AVE S FEDERAL WAY, WA 98003
PHONE: (253) 835-2700
WWW.CITYOFFEDERALWAY.COM

DRAFTED: SMP
DESIGNED: SMP
REVIEWED: DGN
APPROVED:



DRAWING VERSION / REVISION LOG		
NO.	DATE	REVISION

S 288TH ST ROAD DIET - PHASE 2

TRAFFIC CONTROL PLAN - STAGE 4D

CITY PROJECT #:
36238

SHT. **81**
OF **81**

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